

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	RI	FLD-EMRG(003)	2011	3	60

GENERAL NOTES:

- ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, CONDUIT, SIDEWALK, FENCES, ETC., CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
- THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.06 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT OBLITERATED BEFORE CONTROL POINTS ARE LOCATED AND CONSTRUCTION LAYOUT IS ESTABLISHED. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING HIM TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE RESIDENT ENGINEER WILL NOT AUTHORIZE CONSTRUCTION ACTIVITIES TO BEGIN UNTIL HE IS SATISFIED THAT ALL GROUND CONTROL HAS BEEN ESTABLISHED, TIED DOWN, AND DULY RECORDED IN STANDARD FIELD BOOKS.
- ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
- THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE AS DIRECTED BY THE ENGINEER.
- ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANED PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 3 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE PAID FOR UNDER THE CONTRACT UNIT BID PRICE FOR CODE 403.0300 "ASPHALT EMULSION TACK COAT."
- THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT HIS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED WITH THE PERMISSION OF THE ENGINEER.
- UNDER NO CIRCUMSTANCE WILL THE CONTRACTOR BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
- CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
- PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE APPROVED OR MODIFIED BY THE ENGINEER.
- THE COORDINATE SYSTEM IS THE R.I. STANDARD GRID SYSTEM, NAD 83. THE VERTICAL CONTROL IS NGVD 29.
- PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY-TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE STATE.
- NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO EXISTING DRAINAGE STRUCTURES HAS BEEN REESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; IT SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
- ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
- THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM), AND/OR THE ARMY CORPS OF ENGINEERS (ACOE), AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC). COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
- THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

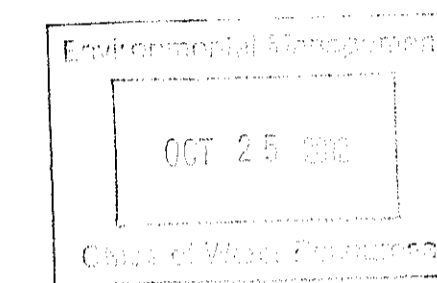
- FOR ALL PROJECTS WITH AT LEAST ONE(1) ACRE OF SOIL DISTURBANCE, R.I.D.O.T. IS REQUIRED TO DEVELOP AND ENFORCE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIPDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE SPECIFIC SWPPP FOR THIS PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- NO UNDISTURBED AREAS SHALL BE CLEARED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE RESTABILIZED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
- STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODABLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH R.I. STD. 9.1.0 TO STABILIZE.
- IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION DEWATERING BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. OFFICE OF ENVIRONMENTAL PROGRAMS.
- JUTE MESH SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
- SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - SEEDING TYPE I.
 - ADHESIVE MULCH STABILIZER
- UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
- PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION. WORK CAN COMMENCE ONLY UPON THE ENGINEER'S AUTHORIZATION.
- ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER SHUTDOWN.
- DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING DRAINAGE AND RUNOFF FLOW DURING STORMS AND PERIODS OF RAINFALL THROUGHOUT THE WORK AREA.
- CATCH BASIN RIM GRADES NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
- PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL BE KEPT TO A MINIMUM.
 - ANY VEGETATIVE CLEARING SHALL BE LIMITED TO BRUSH AND TREES LESS THAN 3" DIAMETER.
 - NO HEAVY EQUIPMENT MAY ENCROACH UPON VEGETATED PERIMETER OR RIVERBANK WETLANDS AS WELL AS BIOLOGICAL WETLANDS.
- THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING.
- R.I. STD. 9.8.0 BALED HAY INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
- WHERE BALED HAY INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):

- DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN ELIMINATED, FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED AS DIRECTED BY THE ENGINEER. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION REQUIRED TO RESOLVE SUCH ISSUES SHALL BE COMPLETED BY THE CONTRACTOR.
- THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
- PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
- ALL HAY BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
- THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND HE SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
- THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ADDITIONAL EROSION CONTROLS, SHALL BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.

UTILITY NOTES:

- EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
- THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39-1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
- ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
- EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW WATER MAINS.
- UTILITY SERVICE CONNECTIONS SHALL BE MAINTAINED TO ALL EXISTING FACILITIES TO REMAIN.
- FIRE HYDRANTS SHALL NOT BE REMOVED FROM SERVICE WITHOUT WRITTEN AUTHORIZATION FROM THE FIRE DEPARTMENT OR THE WATER AUTHORITY.
- ALL NEW WATER LINES SHALL BE DISINFECTED TO THE SATISFACTION OF THE WATER AUTHORITY IN ACCORDANCE WITH THE SPECIFICATIONS.
- ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.



DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
APPROVED WITH CONDITIONS
AS SPECIFIED IN THE LETTER OF APPROVAL
DATED DEC. 18 2012 FILE # 12-046
NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
APPROVED PLANS MUST BE AT CONSTRUCTION SITE

Martin D. Wenzel

REVISIONS			RHODE ISLAND DEPARTMENT OF TRANSPORTATION	
NO.	DATE	BY	STATEWIDE SCOUR REMEDIATION CONTRACT #2	
			STATEWIDE	RHODE ISLAND
STANDARD NOTES 1				
CHECKED BY <u>BMF</u> DATE <u>10/15/12</u> SCALE <u>NO SCALE</u>				

COLLINS ENGINEERS
101 HAMMER MILL ROAD
ROCKY HILL, CT 06067
(860) 571.0384

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	RI	FLD-EMRG (003)	2011	4	60

LANDSCAPE NOTES:

- ALL PLANT MATERIAL MUST BE TAGGED AT THE NURSERY (A RECOGNIZED GROWER OF PLANT MATERIAL) IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE NURSERY GROWN; NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
- ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
- A R.I.D.O.T. LANDSCAPE REPRESENTATIVE MUST BE ON SITE TO APPROVE ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
- ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
- PROVIDE A MINIMUM 6'-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.

STRUCTURAL NOTES FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS:

GENERAL

- ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION, OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS (THE "SPECIFICATIONS"), INCLUDING THE LATEST INTERIM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

CONSTRUCTION DRAWINGS AND DETAILS

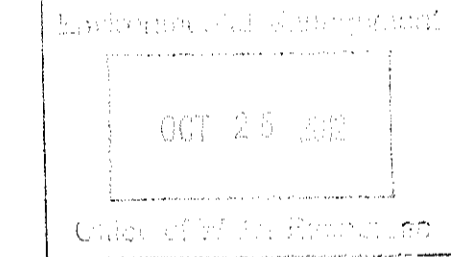
- THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SHOP DRAWINGS IN REFERENCE TO ANCHOR BOLTS:
 - "PRETENSIONING OF ALL ANCHOR NUTS IS REQUIRED, AND SHALL BE ACCOMPLISHED BY TIGHTENING TO 1/6TH TURN BEYOND THE SNUG-TIGHT POSITION."
 - "THE MAXIMUM CLEARANCE BETWEEN THE BOTTOM OF THE LEVELING NUTS AND THE TOP OF THE CONCRETE IS CRITICAL AND SHALL NOT EXCEED THE AMOUNT SPECIFIED ON THIS DRAWING."
- THE USE OF GROUT UNDER BASE PLATES SHALL GENERALLY NOT BE PERMITTED. IF SPECIFIC CONDITIONS WARRANT ITS USE, THE GROUT SHALL NOT BE CONSIDERED LOAD CARRYING; LOADS SHALL BE DIRECTLY SUPPORTED BY THE ANCHOR BOLTS. ADEQUATE DRAINAGE SHALL BE PROVIDED.
- THE DAMPENING EFFECTS OF VIBRATION MITIGATION DEVICES SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SIGNS AND TRAFFIC SIGNALS. IF THE CONTRACTOR CHOOSES TO USE THESE DEVICES FOR WARRANTY PURPOSES, THE TYPE OF DEVICES PROPOSED SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

TRAFFIC SIGNAL NOTES:

- ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE DELIVERED TO THE R.I.D.O.T. MAINTENANCE HEADQUARTERS, 360 LINCOLN AVENUE, WARWICK, RHODE ISLAND, 02888.
- BACK PLATES SHALL BE INSTALLED ON ALL TRAFFIC SIGNAL HEADS.
- THE CONTRACTOR SHALL SUPPLY AND INSTALL ON THE UPPER LEFT HAND CORNER OF THE BACK OF THE CONTROLLER CABINET DOOR A LAMINATED INTERSECTION GRAPHIC AND TABLE DEPICTING THE TRAFFIC DETECTOR RELAY CHANNEL ASSIGNMENTS. THE DIAGRAM SHALL BE A GRAPHIC OF THE INDIVIDUAL INTERSECTION ORIENTED SIMILAR TO THE PLANS SHOWING THE LOCATIONS OF EACH OF THE LOOP DETECTORS. THE DIAGRAM SHALL, AT A MINIMUM, INCLUDE DETECTOR NUMBERS, STREET NAME LABELS, NORTH ARROW, AND CONTROLLER CABINET LOCATION. THE ASSIGNMENT INFORMATION SHALL BE INCLUDED IN A TABLE WHICH SHALL INCLUDE, AT A MINIMUM, THE APPROACH NAME, DETECTOR NUMBER, TERMINAL NUMBER, DETECTOR RACK SLOT NUMBER, RELAY NUMBER, RELAY CHANNEL NUMBER, AND PHASE ASSOCIATED WITH EACH DETECTOR.
- TRAFFIC CONTROLLER CABINETS, UNLESS OTHERWISE NOTED, SHALL BE NEMA TS2 TYPE 1 CABINET SIZE 6 ("T" TYPE) WITH NOMINAL DIMENSIONS OF 52"Hx44"Wx24"D.
- ALL DELAY AND EXTENSION TIMES, AS CALLED FOR ON THE PLANS, FOR PROPOSED LOOP DETECTORS SHALL BE PROGRAMMED IN THE TRAFFIC SIGNAL CONTROLLER AND NOT THE DETECTOR RELAY.
- A BARE GROUND WIRE SHALL BE PLACED IN ALL PVC CONDUITS AND SHALL BE BONDED TO GROUND RODS IN ACCORDANCE WITH SECTION T.03 OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- THE FINAL POSITION OF SIGNAL HEADS, PEDESTRIAN PUSHBUTTONS, DETECTORS, AND STOP LINE AND CROSSWALK PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER IN THE FIELD ACCORDING TO ACTUAL INTERSECTION CHARACTERISTICS.
- A 2' MINIMUM BUFFER SHALL BE PROVIDED BETWEEN THE CURB AND ALL LATERAL OBSTRUCTIONS (INCLUDING ALL SIGNAL POLES AND TRAFFIC/PEDESTRIAN SIGNAL HEADS) TO PROVIDE ADEQUATE CLEARANCE FOR TURNING VEHICLES.
- ALL FOUNDATIONS MUST HAVE CONES OR BARRELS BOLTED TO FOUNDATION BASES UNTIL ACTUAL POLE IS INSTALLED.
- WHEN PLACING TRAFFIC SIGNAL HANDHOLES OR CONDUIT IN EXISTING PORTLAND CEMENT CONCRETE SIDEWALKS, THE ENTIRE SIDEWALK SQUARE OF CONCRETE SHALL BE REPLACED IN ACCORDANCE WITH R.I. STD. 43.1.0. NO PATCHES WILL BE ALLOWED.
- ALL PEDESTRIAN PUSHBUTTONS SHALL BE COMPLIANT WITH "THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES" (ADAAG) AND SHALL INCLUDE A PRESSURE-ACTIVATED (NON-MOVING) BUTTON. SIGNS APPLICABLE TO PUSHBUTTON ACTUATION SHALL BE INSTALLED SUCH THAT THE CROSSING ASSIGNED TO EACH BUTTON IS CLEARLY INDICATED. IF SITE CONDITIONS DO NOT ALLOW PEDESTRIAN PUSHBUTTONS TO BE INSTALLED WHERE CALLED FOR ON THE PLANS, THE R.I.D.O.T. TRAFFIC ENGINEERING UNIT SHALL BE CONSULTED THROUGH AN R.F.I. PRIOR TO INSTALLING THE PUSHBUTTONS. THE FINAL PLACEMENT OF ALL PEDESTRIAN PUSHBUTTONS SHALL BE IN ACCORDANCE WITH ADAAG AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL LOOP DETECTORS SHALL BE CENTERED WITHIN EACH LANE AS DELINEATED, UNLESS OTHERWISE DIMENSIONED ON PLANS.
- ALL LOOP DETECTORS SHALL BE CUT INTO THE FINAL PAVEMENT SURFACE COURSE.
- TRAFFIC SIGNAL CONTROLLERS SHALL BE WIRED SO THAT ANY FIRE PRE-EMPTION SHALL OVERRIDE MANUAL (PUSH BUTTON) OPERATION.
- THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
- ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED, AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
- POLICE OFFICERS (AND NOT FLAGPERSONS) SHALL BE UTILIZED WHEN WORK WILL IMPACT SIGNALIZED INTERSECTIONS AND LIMITED ACCESS HIGHWAYS.
- POLYETHYLENE DRUMS SHALL BE UTILIZED AS A CHANNELIZING DEVICE WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN BEYOND WORKING HOURS WHEN NO WORKERS ARE PRESENT. CONES SHALL BE UTILIZED WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN ONLY DURING WORKING HOURS AND IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
- ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR A MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER WORKZONE TRAFFIC CONTROL DEVICES THAT ARE DAMAGED OR REQUIRE RELOCATION SHALL BE REPLACED AND / OR RELOCATED UNDER THE PAY ITEM FOR "MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION."
- THE PRIVATE VEHICLES OF CONSTRUCTION WORKERS SHALL NOT BE PARKED ON THE TRAVEL LANES OR SHOULDERS. THEY MAY BE PARKED WITHIN THE STATE RIGHT-OF-WAY ONLY IN AREAS 30' BEYOND THE OUTSIDE EDGE OF THE TRAVEL LANES AND/OR IN AREAS APPROVED BY THE ENGINEER.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.
- THE INTENDED VEHICLE PATHS THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES. WATERBORNE PAVEMENT MARKINGS SHALL BE INSTALLED BEFORE THE END OF THE WORK SHIFT ON ALL COLD-PLANNED AND NEW ROADWAY SURFACES THAT WILL BE OPENED TO TRAFFIC AT THE END OF THE SHIFT.



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 OFFICE OF WATER RESOURCES
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 NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
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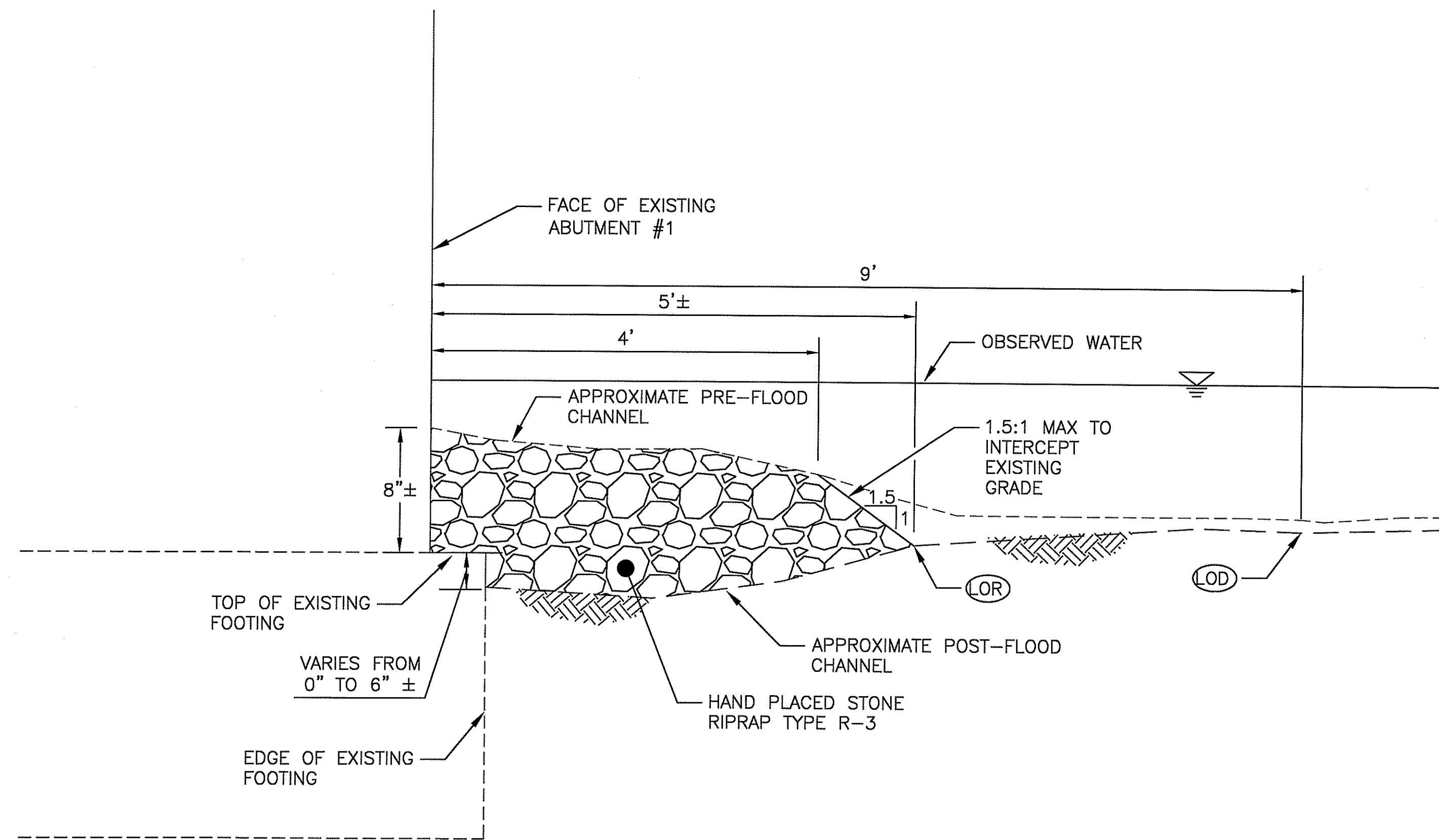
Martin D. Jencel

REVISIONS			RHODE ISLAND DEPARTMENT OF TRANSPORTATION STATEWIDE SCOUR REMEDIATION CONTRACT #2 STATEWIDE RHODE ISLAND
NO.	DATE	BY	
			STANDARD NOTES 2

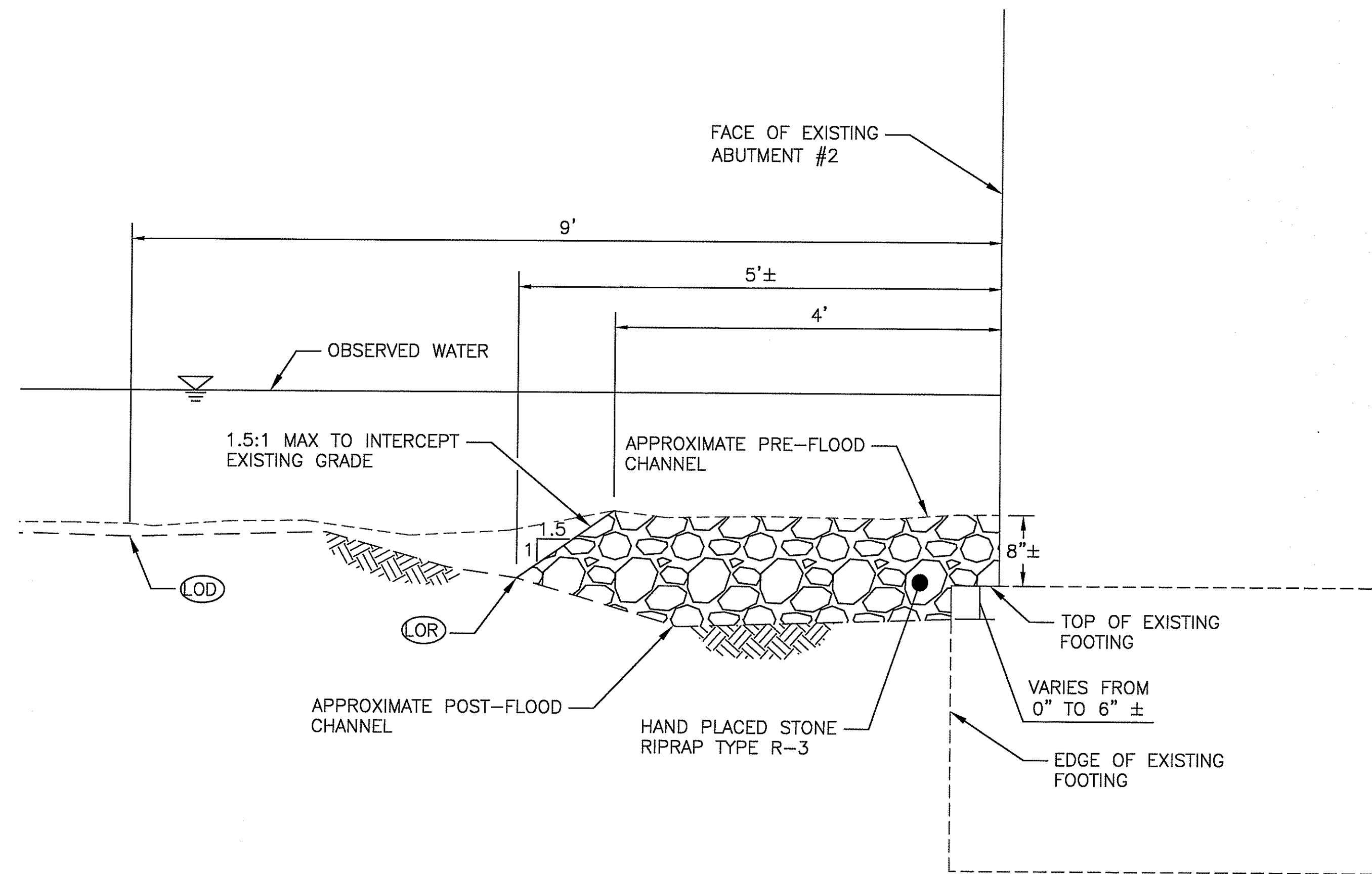
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 ROCKY HILL, CT 06067
 (860) 571.0384

CHECKED BY BMF DATE 10/15/12 SCALE NO SCALE

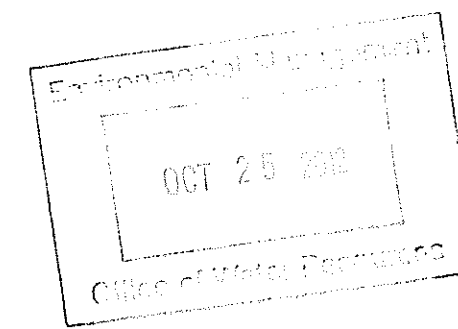
FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	RI	FLD-EMRG (003)	2011	22	60



TYPICAL SECTION ABUTMENT #1
SCALE: 1" = 1'-0"



TYPICAL SECTION ABUTMENT #2
SCALE: 1" = 1'-0"



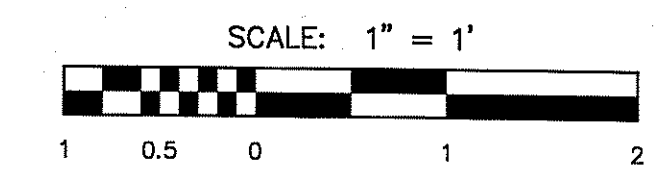
DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
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Martin B. Wenzel

RIPRAP - NOTES:

TOP ELEVATION AND EXTENT OF RIPRAP SHALL NOT EXCEED THAT DEPICTED ON THIS PLAN SHEET. ALL MATERIAL SHALL BE PLACED WITHIN THE LIMIT OF DISTURBANCE, AS DEPICTED HEREIN, TO MATCH ADJACENT EXISTING RIVER BOTTOM AS DIRECTED BY THE ENGINEER. TOE OF RIPRAP SHALL NOT EXTEND BEYOND THE DEPICTED LIMIT OF DISTURBANCE.

REVISIONS			RHODE ISLAND	
NO.	DATE	BY	DEPARTMENT OF TRANSPORTATION	
			STATEWIDE SCOUR REMEDIATION	
			BRIDGE #024901	
			SMITHFIELD	RHODE ISLAND
DETAILS				
CHECKED BY <u>BMF</u> DATE <u>10/15/12</u> SCALE <u>1"=1'</u>				



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FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1	RI	FLD-EMRG (003)	2011	23	60

GENERAL NOTES:

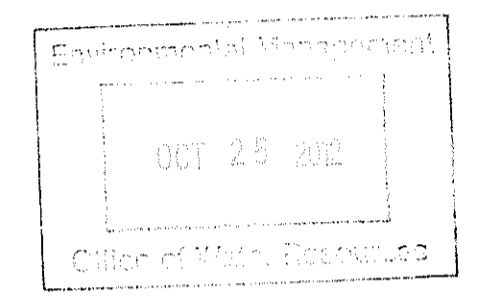
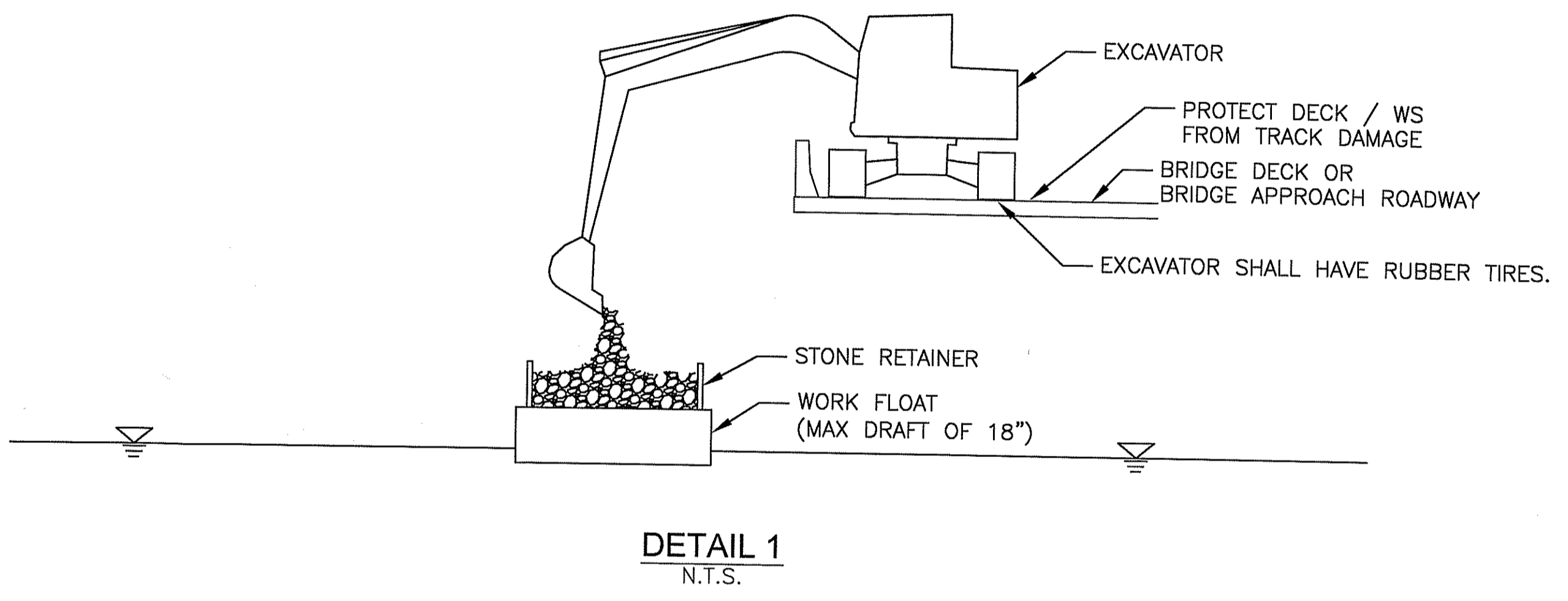
1. THE SCOUR REMEDIATION DESIGN IS BASED ON THE CRITERIA STATED BY THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION. THE CONTRACTOR SHALL NOT FILL ABOVE THE PRE-FLOOD CHANNEL ELEVATION AND SHALL NOT EXCAVATE THE CHANNEL BOTTOM. THE RIPRAP SIZE IS BASED ON EXISTING OBSERVED CONDITIONS AND SIZED TO NOT EXCEED THE DIFFERENCE BETWEEN THE PRE-FLOOD CHANNEL ELEVATION AND THE POST-FLOOD CHANNEL ELEVATION.
2. THE PRE-FLOOD CHANNEL WAS APPROXIMATED USING A COMBINATION OF THE FOLLOWING: HYDROGRAPHIC SURVEY DATA FROM 2010, CROSS SECTION INFORMATION FROM 1996 SCOUR EVALUATIONS, AND ORIGINAL BRIDGE PLANS.
3. THE SCOUR REMEDIATION DESIGN IS BASED UPON THE OBSERVED FIELD CONDITIONS IN OCTOBER 2010 BY COLLINS ENGINEERS, INC. THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING CONDITIONS PRIOR TO STARTING WORK. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION OR THE ENGINEER.
4. THE INTENT OF THE SCOUR REMEDIATION IS TO RESTORE THE CHANNEL ADJACENT TO ABUTMENT #1 AND ABUTMENT #2 TO PRE-FLOOD CONDITIONS.
5. THE CONTRACTOR SHALL NOT DREDGE OR EXCAVATE THE CHANNEL OR FILL ABOVE THE PRE-FLOOD ELEVATION OR OUTSIDE THE LIMIT OF DISTURBANCE.
6. CONTRACTOR IS ADVISED THAT CONSTRUCTION ACTIVITIES OTHER THAN HAND REMOVAL OF DEBRIS ARE PROHIBITED FROM TAKING PLACE WITHIN VEGETATED RIVERBANK WETLANDS ASSOCIATED WITH THE RIVER (200 FEET FROM EITHER EDGE OF THE CHANNEL). ALL MOBILIZATION OF MATERIALS AND EQUIPMENT MUST TAKE PLACE FROM THE BRIDGE DECK OR BRIDGE APPROACH ROADWAY. IN ADDITION, THE CONTRACTOR IS PROHIBITED FROM PLACING ANY EQUIPMENT DIRECTLY ON OR INTO THE STREAM BOTTOM. NO TREE-TRIMMING OR CLEARING OF ANY WOODY VEGETATION IS PERMITTED TO OCCUR.
7. HYDROGRAPHIC CONTOURS ARE BELOW THE DATUM ELEVATION AND ARE SHOWN FOR CONTRACTOR INFORMATION ONLY. THE HYDROGRAPHIC DATUM (DATUM ELEVATION 0.0) IS LOCATED AT THE BOTTOM OF THE SLAB AT THE DOWNSTREAM (EAST) END OF ABUTMENT #1.
8. THE APPROXIMATE 100-YEAR FLOOD ELEVATION WAS INTERPRETED FROM THE LATEST FLOOD EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAPS (COMMUNITY # 44007C-0168-G MARCH 02, 2009). THE 100-YEAR FLOOD ELEVATION IS REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988.
9. THE CONSTRUCTION SEQUENCE SHOWN ON THIS SHEET IS SUGGESTED ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEANS AND METHODS OF CONSTRUCTION, INCLUDING OBTAINING ANY REQUIRED ENVIRONMENTAL PERMITS, AND/OR MODIFICATIONS TO EXISTING PERMITS, AT THE SOLE EXPENSE OF THE CONTRACTOR. ALL REQUIRED APPLICATIONS FOR SUCH PERMITS AND/OR MODIFICATIONS INVOLVING ANY PROPERTY OWNED OR CONTROLLED BY THE STATE OF RHODE ISLAND, SHALL BE COORDINATED THROUGH THE ENGINEER, AND RIDOT'S NATURAL RESOURCES UNIT, PRIOR TO SUBMISSION TO ANY REVIEWING AGENCY.
10. CONTRACTOR IS ADVISED THAT MATERIAL PLACED IN ACCORDANCE WITH THE SUGGESTED CONSTRUCTION SEQUENCE CONTAINED HEREIN MAY REQUIRE EITHER A "LOW FLOW" CONDITION (IN THE EVENT THAT WATER DEPTHS DURING "NORMAL FLOW" OR "HIGH FLOW" CONDITIONS PRECLUDE OBSERVATION OF MATERIAL PLACEMENT OPERATIONS BY THE ENGINEER) OR A "HIGH FLOW" CONDITION (IN ORDER TO OBTAIN THE DRAFT REQUIRED FOR A WORK FLOAT OR WORK BARGE). THE CONTRACTOR SHOULD BE AWARE THAT IN ORDER TO COMPLETE ALL WORK WITHIN THIS CONTRACT, FLOW CONDITIONS STATED ABOVE MAY NECESSITATE AN EXTENDED DURATION OF CONSTRUCTION SPANNING MULTIPLE SEASONAL FLOW CONDITIONS.
11. THE CONTRACTOR CAN CONNECT OR TIE INTO THE BRIDGE STRUCTURE (PIER COLUMNS, PIER CAP, ETC) WITH LINES AND PULLIES FOR THE SOLE PURPOSE OF ASSISTING THE WORK FLOAT, PROVIDING THAT THE MEANS AND METHODS ARE NON-DESTRUCTIVE TO THE BRIDGE STRUCTURES.
12. THE CONTRACTOR SHALL PROVIDE PROTECTION BETWEEN THE BRIDGE PAVEMENT AND THE EXCAVATOR.

SUGGESTED WORK SEQUENCE:

1. PICK WORK FLOAT WITH CRANE OR EXCAVATOR (WITH RUBBER TIRES) LOCATED ON BRIDGE DECK OR BRIDGE APPROACH ROADWAY AND PLACE IN WATERWAY. SECURE WORK BARGE WITH TAG LINES AS NECESSARY.
2. DELIVER RIPRAP MATERIAL IN LANE CLOSURE ON BRIDGE DECK. PLACE RIPRAP MATERIAL ON WORK FLOAT WITH EXCAVATOR POSITIONED ON BRIDGE DECK (SEE DETAIL 1).
3. **Float work float into appropriate location and install riprap to limits indicated on plan and sections. Material shall be hand placed from work float.**
4. THE CONTRACTOR IS ADVISED THAT THE SUGGESTED CONSTRUCTION SEQUENCE CONTAINED HEREIN IS BASED UPON SITE CONDITIONS OBSERVED DURING THE DESIGN PHASE, AND THAT THE ACTUAL SITE CONDITIONS AT THE TIME OF CONSTRUCTION MAY DIFFER IN TERMS OF MEANS AND METHODS THAT MAY BE AVAILABLE BASED UPON THE STREAM FLOW AND OTHER FACTORS (INCLUDING LIMITED HEADROOM UNDER THE BRIDGE STRUCTURE).
5. REMOVE WORK FLOAT WITH CRANE OR EXCAVATOR.

WORK SEQUENCE NOTES:

1. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT NO SPOIL AREA FOR SURPLUS MATERIALS IS AVAILABLE FOR THIS CONTRACT WITHIN THE STATE'S RIGHT OF WAY. THEREFORE, ALL MATERIAL TO BE REMOVED FROM THE JOB SITE (INCLUDING REMOVAL OF DEBRIS ADJACENT TO BRIDGE IF PART OF SCOPE) SHALL BE DISPOSED OF BY THE CONTRACTOR OFF THE STATE'S PROPERTY IN ACCORDANCE WITH ALL STATE, FEDERAL, AND LOCAL LAWS. ALL COSTS ASSOCIATED WITH THE SPOIL AREA AND REMOVAL OF SPOIL MATERIAL AND DEBRIS SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.
2. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE AND ADVISE THE STATE OF THE TYPE, SIZE, AND WEIGHT OF ALL VEHICLES THE CONTRACTOR INTENDS TO USE ON THE THE STRUCTURE(S) DURING CONSTRUCTION BASED ON THE CONDITION OF THE EXISTING STRUCTURE(S). THIS DETERMINATION SHALL BE MADE BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF RHODE ISLAND EMPLOYED AND PAID BY THE CONTRACTOR. THE DETERMINATION BY THIS PROFESSIONAL ENGINEER IS TO BE SUBMITTED TO THE STATE 14 DAYS PRIOR TO THE USE OF ANY VEHICLES ON THE STRUCTURE(S) WITH ALL RESTRICTIONS ENUMERATED BY THE CONTRACTOR BEING STRICTLY ADHERED TO. IN THE EVENT THAT THE CONTRACTOR/SUBCONTRACTOR FAILS TO COMPLY WITH THE INSTRUCTIONS OF THE PROFESSIONAL ENGINEER FOR THE USE OF ANY VEHICLE, THE WORK WILL BE IMMEDIATELY SUSPENDED UNTIL CORRECTIVE PROCEDURES SATISFACTORY TO THE PROFESSIONAL ENGINEER AND THE STATE ARE EMPLOYED. COSTS OF ALL DAMAGE, DIRECT OR INDIRECT, SHALL BE BORNE AND SUSTAINED BY THE CONTRACTOR. MATERIALS NOT TO BE DUMPED DIRECTLY ON TO THE BRIDGE DECK. MATERIALS NOT TO BE DUMPED DIRECTLY ON TO THE ROADWAY SURFACE.
3. THIS BRIDGE HAS BEEN LOAD RATED AND POSTED FOR NO BLANKET PERMIT.
4. CONTRACTOR IS ADVISED THAT CONSTRUCTION ACTIVITIES OTHER THAN HAND REMOVAL OF DEBRIS ARE PROHIBITED FROM TAKING PLACE WITHIN VEGETATED RIVERBANK WETLANDS ASSOCIATED WITH THE RIVER (200 FEET FROM EITHER EDGE OF THE CHANNEL). ALL MOBILIZATION OF MATERIALS AND EQUIPMENT MUST TAKE PLACE FROM THE BRIDGE DECK OR BRIDGE APPROACH ROADWAY. IN ADDITION, THE CONTRACTOR IS PROHIBITED FROM PLACING ANY EQUIPMENT DIRECTLY ON OR INTO THE STREAM BOTTOM. NO TREE-TRIMMING OR CLEARING OF ANY WOODY VEGETATION IS PERMITTED TO OCCUR.



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NO.	DATE	BY	
			STATEWIDE SCOUR REMEDIATION BRIDGE #024901
			SMITHFIELD RHODE ISLAND
			GENERAL NOTES & SUGGESTED CONSTRUCTION SEQUENCE
			CHECKED BY <u>BMF</u> DATE <u>10/15/12</u> SCALE <u>NO SCALE</u>

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