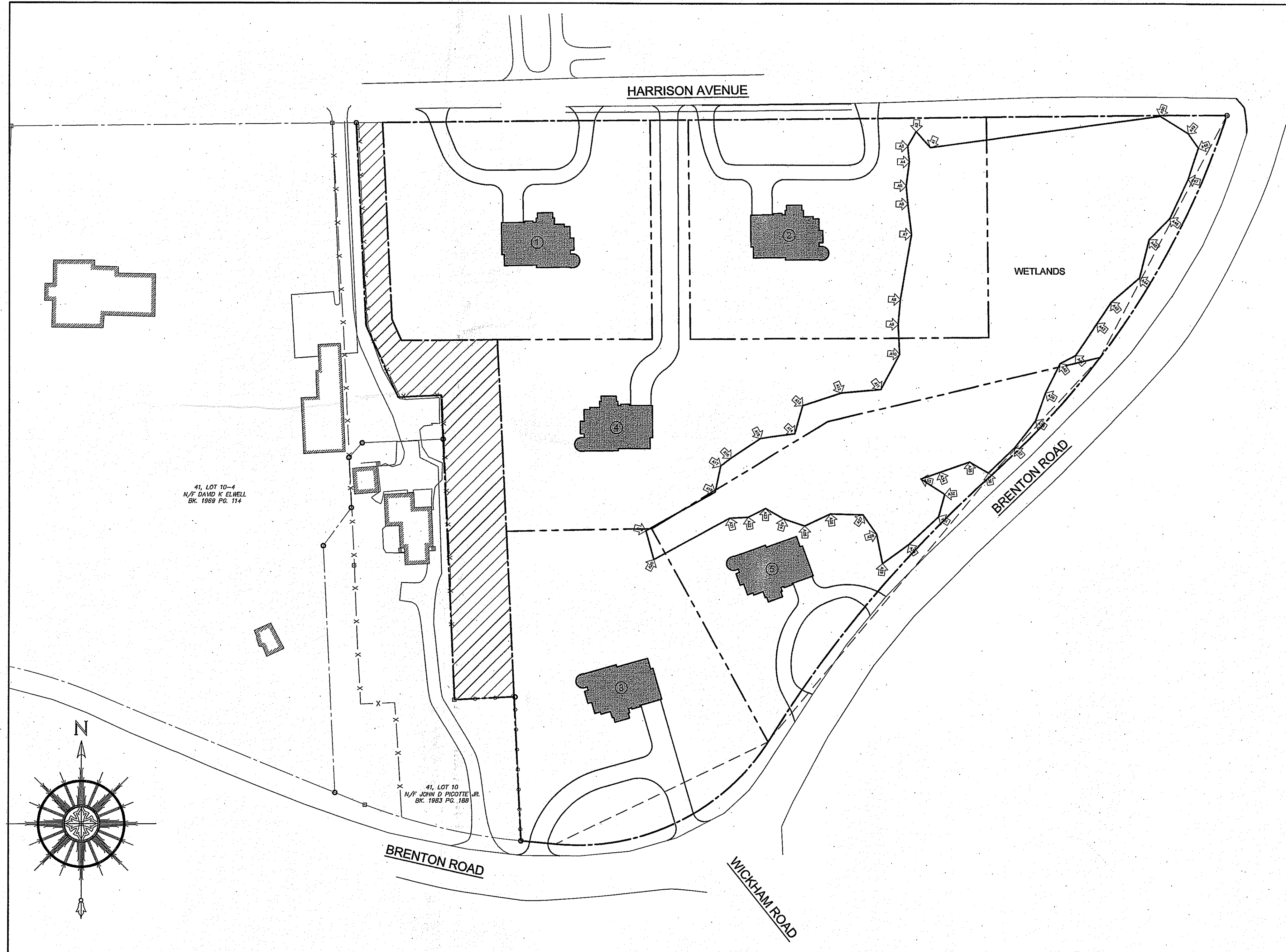


SCHOOLYARD PROPERTIES

RESIDENTIAL SUBDIVISION

A.P. 41 LOT 14

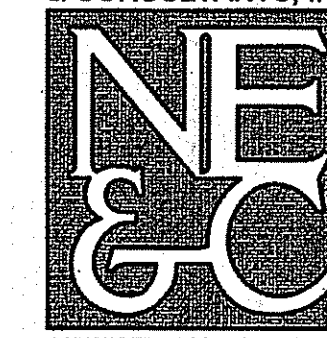
HARRISON AVENUE AND BRENTON ROAD
NEWPORT, RHODE ISLAND



SITE PLAN

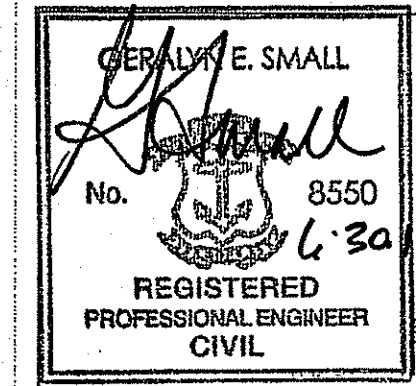
SCALE = 1" = 50'

ENGINEER: NORTHEAST ENGINEERS & CONSULTANTS, INC.



SITE/CIVIL
LAND PLANNING
WATERFRONT
SURVEYING
GEOTECHNICAL
ENVIRONMENTAL
TRANSPORTATION
STRUCTURAL
MATERIALS TESTING

A KNOWLEDGE CORPORATION
65 JOHN CLARKE ROAD MIDDLETOWN RI 02842
PHONE (401) 849-0810 FAX (401) 846-4169
WWW.NORTHEASTENGINEERS.COM



ENGINEER CERTIFICATION

OWNER:

SCHOOLYARD PROPERTIES LLC
C/O JAY PICOTTE
19 BRENTON ROAD
NEWPORT, RI 02840

DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO.: 17-043
DATED AUG 24 2017
SEE LETTER OF SAME DATE.

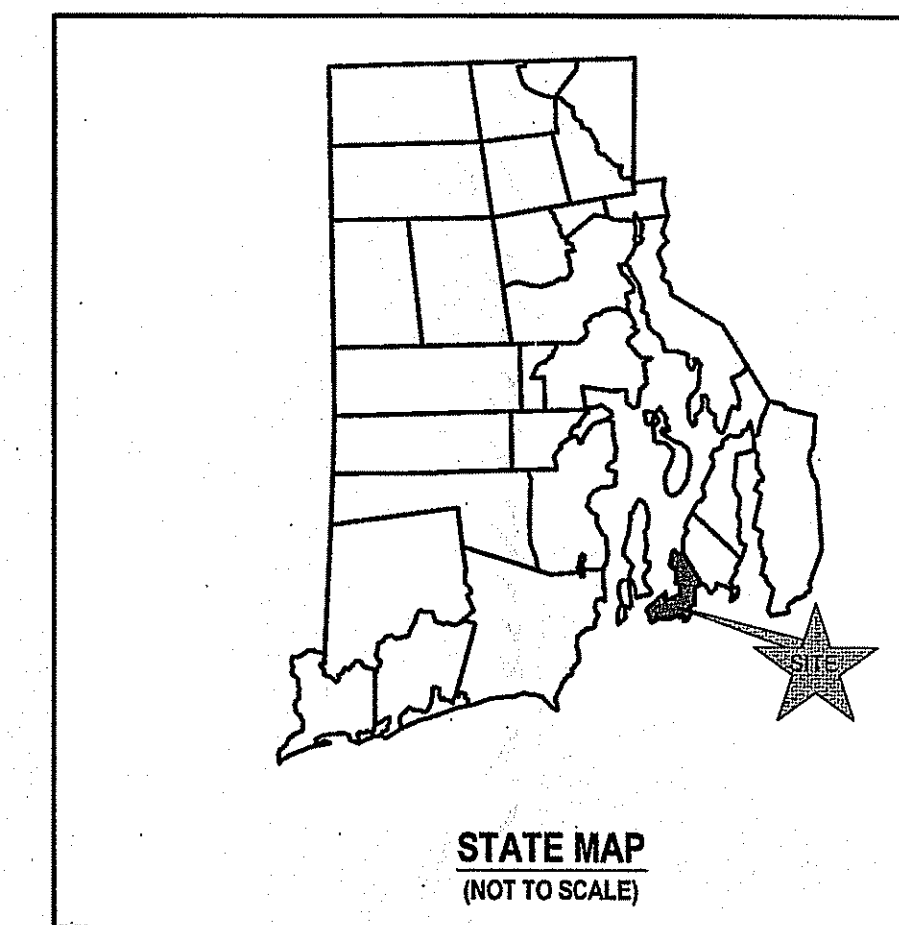
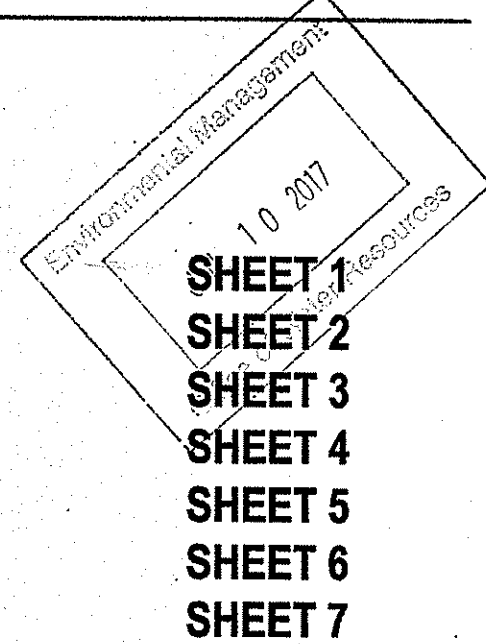
Cynthia D. Wenzel

APRIL 13, 2017 PERMIT SET

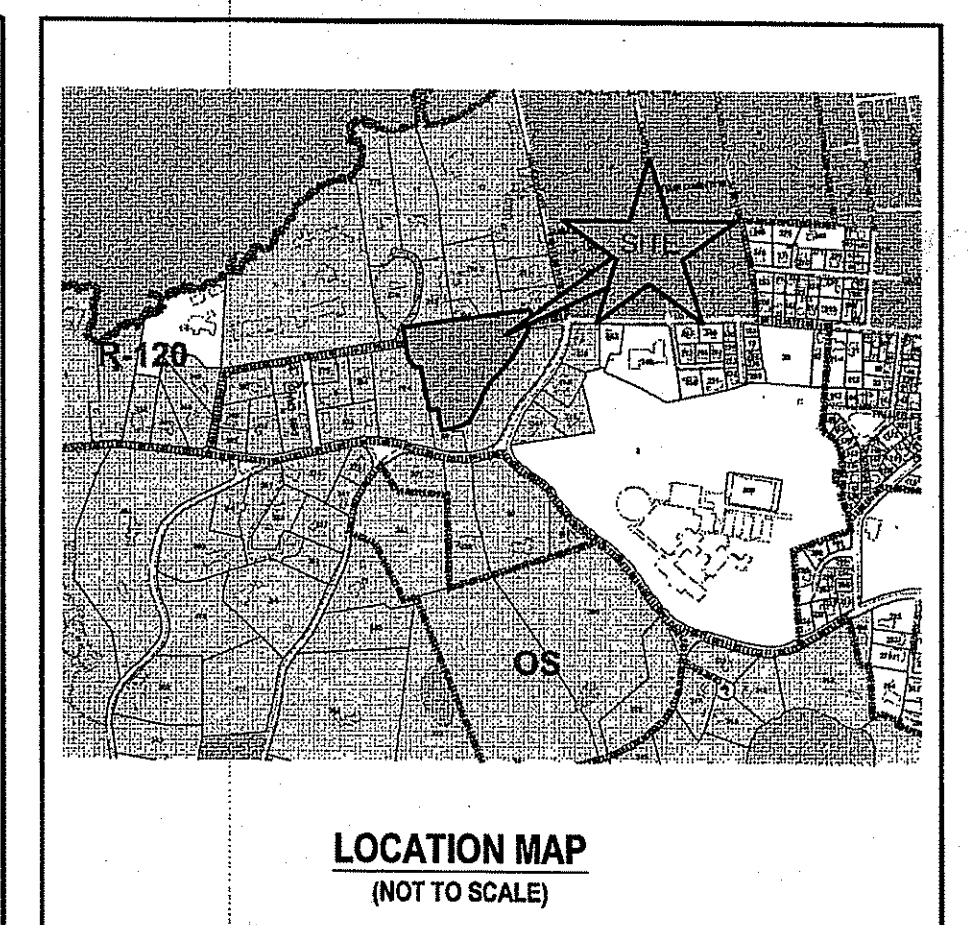
PLAN INDEX

SITE/CIVIL ENGINEERING PLANS

- TITLE SHEET
- SITE/CIVIL NOTES
- EXISTING CONDITIONS
- SUBDIVISION PLAN
- DRAINAGE AND UTILITY PLAN
- CONSTRUCTION GRADING AND SESC PLAN
- PROPOSED SITE / CIVIL DETAILS



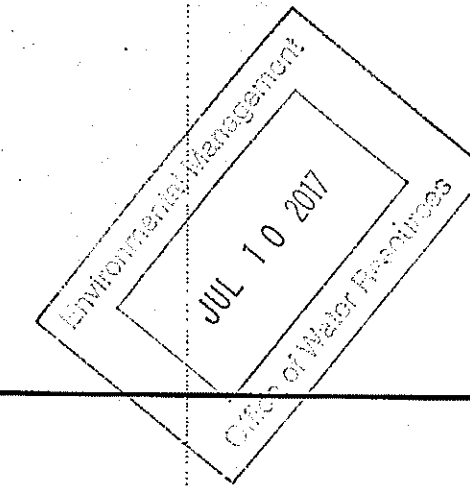
STATE MAP
(NOT TO SCALE)



LOCATION MAP
(NOT TO SCALE)

SUBMISSION AND REVISION SUMMARY

AGENCY OR REVISION	DATE:	COMMENTS:



GENERAL NOTES

- PROPERTY LINE SURVEY PERFORMED BY NORTHEAST ENGINEERS & CONSULTANTS, INC. IN SEPTEMBER 2016.
- EXISTING CONDITIONS, RAZED BUILDINGS, AND WETLANDS TAKEN FROM PLAN ENTITLED, "EXISTING CONDITIONS SITE PLAN, 90 HARRISON AVENUE, ASSESSOR'S MAP 41, PARCEL 14, NEWPORT, RHODE ISLAND", PREPARED BY LAND DEVELOPMENT ENGINEERING & CONSULTING, LLC, DATED JULY 15, 2015.
- TOPOGRAPHY TAKEN FROM PLAN REFERENCED IN NOTE 2 ABOVE AND SUPPLEMENTED WITH FIELD OBSERVATIONS BY NORTHEAST ENGINEERS & CONSULTANTS, INC. ON SEPTEMBER 22, 2016.
- ON-SITE FRESHWATER WETLANDS FLAGGED BY NATURAL RESOURCE SERVICES IN AUGUST 2015.
- ELEVATIONS REFERENCE NAVD83.
- NORTH ARROW REFERENCES MAGNETIC NORTH (SEPT. 2016).
- EXISTING SOILS SCALED FROM NATURAL RESOURCE CONSERVATION SOIL SURVEY AND SHOULD BE CONSIDERED APPROXIMATE.
- THE SITE IS NOT LOCATED WITHIN ANY KNOWN FLOOD HAZARD ZONES AS SHOWN ON THE FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP COMMUNITY-PANEL NUMBER 44050C0177J, DATED SEPTEMBER 4, 2013.
- SOIL CHARACTERISTICS OF THE SITE ARE CATEGORY "Pm1" AND "Pm2", PITTSFORD SILT LOAM AND "C&C", CANTON AND CHARLTON PER THE NATURAL RESOURCE CONSERVATION SOIL SURVEY. THESE ARE TYPE "C" AND "B" HYDROLOGIC SOILS, RESPECTIVELY.
- THE CONTRACTOR SHALL VERIFY THE PROPOSED LAYOUT AND DETAILS WITH THEIR RELATIONSHIP TO THE EXISTING SITE SURVEY. CONTRACTOR SHALL ALSO VERIFY ALL DIMENSIONS, SITE CONDITIONS AND MATERIAL SPECIFICATIONS AND SHALL NOTIFY THE OWNER AND ENGINEER OF ANY ERRORS, OMISSIONS OR DISCREPANCIES BEFORE COMMENCING WORK.
- THE UNDERGROUND UTILITIES KNOWN TO EXIST BY THE ENGINEER FROM HIS SEARCH OF RECORDS ARE INDICATED ON THE PLANS. CONTRACTOR SHALL VERIFY THE LOCATIONS AND DEPTHS OF THE UTILITIES AND EXERCISE PROPER CARE IN EXCAVATING IN THE AREA. ALL DAMAGED PORTIONS SHALL BE REPLACED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE AFFECTED UTILITY COMPANY AND SHALL BE THE CONTRACTOR'S RESPONSIBILITY. PERSONAL INJURY RESULTING FROM CONTACT WITH EXISTING UTILITIES SHALL BE THE CONTRACTOR'S RESPONSIBILITY. WHEREVER CONNECTION OF NEW UTILITIES TO EXISTING UTILITIES ARE SHOWN ON THE PLANS, THE CONTRACTOR SHALL EXPOSE THE EXISTING LINES AT THE PROPOSED CONNECTIONS TO VERIFY THEIR LOCATIONS AND DEPTHS PRIOR TO EXCAVATION FOR NEW LINES. (PLEASE CALL DIG SAFE PRIOR TO CONSTRUCTION AT 1-888-DIG-SAFE AND ALL LOCAL UTILITY COMPANIES.)
- THE CONTRACTOR SHALL NOTIFY ALL AGENCIES TO VERIFY THE ACTUAL LOCATIONS OF ALL UTILITIES IN THE PROJECT AREA PRIOR TO EXCAVATING.
- THE CONTRACTOR SHALL RESTORE TO THEIR ORIGINAL CONDITION OR BETTER, ALL IMPROVEMENTS DAMAGED AS A RESULT OF THE CONSTRUCTION, INCLUDING PAVEMENTS, EMBANKMENTS, CURBS, SIGNS, LANDSCAPING, STRUCTURES, UTILITIES, WALLS, FENCES, ETC. UNLESS PROVIDED FOR SPECIFICALLY IN THE PROPOSAL.
- CONTRACTOR SHALL EXERCISE EXTREME CAUTION TO PRESERVE STREET MONUMENTS.
- STREET MONUMENTS THAT ARE DISTURBED SHALL BE RESTORED UNDER THE LICENSED LAND SURVEYOR'S DIRECTION. ANY NEW DATA SUCH AS ELEVATIONS SHALL BE CERTIFIED BY THE SURVEYOR, AND SUBMITTED TO THE CITY OF NEWPORT.
- DEVIATIONS OR CHANGES FROM THESE PLANS WILL NOT BE ALLOWED UNLESS APPROVED BY THE PROJECT ENGINEER, APPROPRIATE AGENCY AND OWNER.
- RELOCATION OF ANY UTILITIES SHALL BE AT THE OWNERS EXPENSE AND BE COMPLETED WITH THE UTILITY WORK. THE OWNER SHALL BE NOTIFIED AS TO THE RELOCATION REQUIRED PRIOR TO THE START OF CONSTRUCTION.
- AN APPROVED SET OF PLANS AND ALL APPLICABLE PERMITS MUST BE AVAILABLE AT THE CONSTRUCTION SITE AT ALL TIMES.
- CONTRACTOR AGREES THAT HE/SHE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF THE CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT TO BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEERS HARMLESS FROM ANY AND ALL LIABILITY, REAL AND ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR PROJECT ENGINEER.
- ALL TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION INCLUDING ALL REVISIONS.
- AT THE TIME OF DEVELOPMENT OF EACH LOT THE DEVELOPER WILL BE REQUIRED TO COMPLY WITH ALL APPLICABLE PROVISIONS OF THE CITY SOIL EROSION & SEDIMENT CONTROL ORDINANCE (CITY CODE CHAPTER 15.26), INCLUDING COMPLIANCE WITH THE APPROVED STORM WATER MANAGEMENT PLAN FOR THE SUBDIVISION. A NOTE TO THIS EFFECT MUST BE ADDED TO THE FINAL PLAN PRIOR TO RECORDING.

GRADING NOTES

- ADEQUATE PROVISIONS SHALL BE MADE TO PREVENT SURFACE WATERS FROM DAMAGING THE CUT FACE OF AN EXCAVATION OR THE SLOPED SURFACES OF A FILL. FURTHERMORE, ADEQUATE PROVISIONS SHALL BE MADE TO PREVENT SEDIMENT RUNOFF FROM LEAVING THE SITE.
- ANY GRADED AREAS SHALL BE SODDED OR PLANTED IMMEDIATELY AFTER THE GRUBBING WORK HAS BEEN COMPLETED.
- THE CITY SHALL BE INFORMED OF THE LOCATION OF THE DISPOSAL SITE, IF ANY, FOR THE PROJECT.
- NO GRADING WORK SHALL BE DONE ON SATURDAYS, SUNDAYS AND HOLIDAYS AT ANY TIME WITHOUT PRIOR NOTICE TO THE CITY, PROVIDED SUCH GRADING WORK IS ALSO IN CONFORMANCE WITH THE COMMUNITY NOISE CONTROL STANDARDS.
- ALL GRADING OPERATIONS SHALL BE PERFORMED IN CONFORMANCE WITH THE APPLICABLE PROVISIONS OF THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT AND THE CITY.
- WHERE APPLICABLE AND FEASIBLE THE MEASURES TO CONTROL EROSION AND OTHER POLLUTANTS SHALL BE IN PLACE BEFORE GRADING WORK IS INITIATED.
- TEMPORARY EROSION CONTROLS SHALL NOT BE REMOVED BEFORE PERMANENT EROSION CONTROLS ARE IN-PLACE AND ESTABLISHED.
- IF THE GRADING WORK INVOLVES CONTAMINATED SOIL, THEN ALL GRADING WORK SHALL BE DONE IN CONFORMANCE WITH APPLICABLE STATE AND FEDERAL REQUIREMENTS.
- NONCOMPLIANCE TO ANY OF THE ABOVE REQUIREMENTS SHALL MEAN IMMEDIATE SUSPENSION OF ALL WORK, AND REMEDIAL WORK SHALL COMMENCE IMMEDIATELY. ALL COSTS INCURRED SHALL BE BILLED TO THE VIOLATOR. FURTHERMORE, VIOLATORS SHALL BE SUBJECT TO ADMINISTRATIVE, CIVIL AND/OR CRIMINAL PENALTIES.

UTILITY NOTES

- EXISTING UTILITIES SHOWN IN HARRISON AVENUE HAVE BEEN SCALED FROM PLAN TITLED, "UPGRADE/IMPROVEMENTS TO SANITARY SEWER SYSTEM" PRODUCED BY WESTON & SAMPSON ENGINEERS, INC., LAST REVISED OCTOBER 1998.
- EXISTING DRAINAGE INFORMATION SCALED FROM PLAN TITLED, "EXISTING PRIVATE SEWER SYSTEM DISCHARGING INTO BRENTON COVE" PROVIDED BY THE CITY OF NEWPORT DEPARTMENT OF ENGINEERING AND INSPECTION, DATED JULY 24, 1989, AND SUPPLEMENTED WITH FIELD LOCATIONS DETERMINED BY NE&C.
- EXISTING GAS MAIN INFORMATION IN BRENTON ROAD TAKEN FROM PLAN PROVIDED BY NATIONAL GRID ON DECEMBER 16, 2016.
- UTILITIES SHOWN WITHIN SITE TAKEN FROM PLAN ENTITLED "EXISTING CONDITIONS SITE PLAN, ASSESSOR'S MAP 41, PARCEL 14, 90 HARRISON AVENUE, NEWPORT, RHODE ISLAND", PREPARED BY LAND DEVELOPMENT ENGINEERING & CONSULTING, LLC, DATED JULY 15, 2015 AND SUPPLEMENTED WITH FIELD OBSERVATIONS BY NE&C.
- THE LOCATIONS OF PROPOSED ELECTRICAL CONDUITS AND CONNECTIONS TO THE EXISTING OVERHEAD SERVICES IN HARRISON AVENUE AND BRENTON ROAD ARE PRELIMINARY. FINAL DESIGN OF THE ELECTRICAL SERVICES ARE SUBJECT TO DESIGN REVIEW AND APPROVAL BY NATIONAL GRID.
- THE LOCATIONS OF WATER LINE SERVICES, AND METER PIT ARE PRELIMINARY AND SUBJECT TO REVIEW AND APPROVAL BY NEWPORT WATER.
- PROPOSED SEWER LINES SUBJECT TO REVIEW BY THE CITY OF NEWPORT.
- ELECTRIC, TELEPHONE AND CABLE SERVICES SHALL BE INSTALLED UNDERGROUND.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ALL ASSUMPTIONS, DEDUCTIONS, OR CONCLUSIONS HE/SHE MAY MAKE OR DERIVE FROM THE SUBSURFACE INFORMATION OR DATA FURNISHED ON THE PLANS. THE CONTRACTOR MUST SATISFY HIMSELF/HERSELF THROUGH HIS/HER OWN INVESTIGATIONS AS TO WHAT SUBSURFACE CONDITIONS ARE TO BE ENCOUNTERED.
- IF THE CONTRACTOR ELECTS NOT TO EXPOSE AND VERIFY ALL EXISTING UNDERGROUND UTILITIES AND STRUCTURES AT CROSSINGS PRIOR TO PIPELINE EXCAVATION, HE/SHE FORFEITS HIS/HER RIGHTS FOR ANY CLAIMS FOR COMPENSATION CAUSED BY ANY CONFLICTS WITH EXISTING UTILITIES AND STRUCTURES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER DISPOSAL OF ALL EFFLUENT ASSOCIATED WITH THE CONSTRUCTION ACTIVITY AND THE DISINFECTION AND HYDROTESTING OPERATIONS TO SAFEGUARD PUBLIC HEALTH AND SAFETY IN ACCORDANCE WITH APPLICABLE DEPARTMENT OF HEALTH REQUIREMENTS. ALL PERMITS AND LICENSES FOR CONSTRUCTION WATER DISPOSAL, INCLUDING ALL APPLICATIONS, CHARGES, FEES, AND TAXES, ARE THE RESPONSIBILITY OF THE CONTRACTOR.
- APPLICABLE UTILITY EASEMENTS SHALL BE ESTABLISHED PRIOR TO FINAL APPROVAL.
- DIAMETER OF SEWER SERVICES SUBJECT TO DESIGN REVIEW BY NEWPORT D.P.W.

PUBLIC HEALTH SAFETY AND CONVENIENCE NOTES

- CONTRACTOR SHALL OBSERVE AND COMPLY WITH ALL FEDERAL, STATE, AND LOCAL LAWS REQUIRED FOR THE PROTECTION OF PUBLIC HEALTH, SAFETY AND ENVIRONMENTAL QUALITY.
- THE CONTRACTOR AT HIS/HER EXPENSE, SHALL KEEP THE PROJECT AREA AND SURROUNDING AREA FREE FROM RUBBISH, DUST, NOISE, EROSION, ETC. THE WORK SHALL BE DONE IN CONFORMANCE WITH THE AIR AND WATER POLLUTION CONTROL STANDARDS AND REGULATIONS OF ALL APPLICABLE FEDERAL, STATE AND LOCAL AGENCIES.
- NO CONTRACTOR SHALL PERFORM ANY CONSTRUCTION OPERATION SO AS TO CAUSE FALLING ROCKS, SILT OR DEBRIS IN ANY FORM TO FALL, SLIDE OR FLOW ONTO ADJOINING PROPERTIES, STREETS OR NATURAL WATERCOURSES. SHOULD SUCH VIOLATIONS OCCUR, THE CONTRACTOR MAY BE CITED AND THE CONTRACTOR SHALL IMMEDIATELY MAKE ALL REMEDIAL ACTIONS AS NECESSARY.
- THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN ALL NECESSARY SIGNS, LIGHTS, FLARES, BARRICADES, MARKERS, CONES, AND OTHER PROTECTIVE FACILITIES AND SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION, CONVENIENCE AND SAFETY OF THE PUBLIC.

SOIL EROSION AND SEDIMENT CONTROL NOTES

- CONSTRUCTION SEQUENCE:
 - DO NOT BEGIN CONSTRUCTION UNTIL ALL LOCAL, STATE, AND FEDERAL PERMITS HAVE BEEN APPLIED FOR AND RECEIVED.
 - ALL CONSTRUCTION TRAFFIC SHALL ENTER AND EXIT THE SITE VIA THE CONSTRUCTION ENTRANCE SHOWN ON THE PLAN.
 - INSTALL SILT FENCES, SILT SACKS, CHECK DAMS AND/OR STRAW BALES AS INDICATED ON THE DRAWINGS TO CONTROL EROSION AND PREVENT SEDIMENT CONTAMINATION OF WETLANDS PRIOR TO ANY EARTH MOVING ACTIVITIES.
 - REMOVE TREES ONLY WHERE NECESSARY. ALL STUMPS TO BE GROUND OR DISPOSED OF OFF SITE.
 - DEMOLISH EXISTING PAVEMENT IN AREAS OF CONSTRUCTION. IT IS ANTICIPATED THAT ALL PAVEMENT WILL BE REMOVED AS EACH LOT IS DEVELOPED.
 - CONSTRUCT UTILITIES.
- NOTES:
 - DURING CONSTRUCTION AND THEREAFTER, EROSION CONTROL MEASURES ARE TO BE IMPLEMENTED AS NOTED. ONLY THE SMALLEST PRACTICAL AREA OF LAND SHOULD BE EXPOSED AT ANY ONE TIME DURING DEVELOPMENT. WHEN LAND IS EXPOSED DURING DEVELOPMENT, THE EXPOSURE SHOULD BE KEPT TO THE SHORTEST PRACTICAL PERIOD OF TIME.
 - ANY DISTURBED AREAS WHICH ARE TO BE LEFT TEMPORARILY AND WHICH WILL BE REGRADED LATER DURING CONSTRUCTION SHALL BE STABILIZED WITHIN FOURTEEN DAYS IN ACCORDANCE WITH TEMPORARY MEASURES IN THE VEGETATIVE PRACTICE NOTES.
 - AREAS TO BE FILLED SHALL BE CLEARED, GRUBBED, AND STRIPPED OF TOP SOIL TO REMOVE VEGETATION, ROOTS, AND ANY OTHER OBJECTIONABLE MATERIAL.
 - ALL FILL SHALL BE COMPACTED TO 95% MAX. DENSITY IN LAYERS NOT TO EXCEED 12" IN THICKNESS TO REDUCE EROSION, SLIPPAGE, SETTLEMENT SUBSIDENCE, OR OTHER RELATED PROBLEMS.
 - FILL INTENDED TO SUPPORT BUILDING STRUCTURES AND CONDUITS, ETC., SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL CODES AND SPECIFICATIONS.
 - FILL MATERIAL SHALL BE FREE OF BRUSH, RUBBISH, ROCKS, LOGS, STUMPS, BUILDING DEBRIS, AND OTHER OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
 - FROZEN, SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIAL SHALL NOT BE INCORPORATED INTO FILLS.
 - FILL SHALL NOT BE PLACED ON A FROZEN FOUNDATION SUBGRADE.
 - SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ATTENTION OF THE DESIGN ENGINEER.
 - ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 14 DAYS OF FINISH GRADING IN ACCORDANCE WITH THE VEGETATIVE PRACTICE NOTES.
 - REMOVE TEMPORARY EROSION CONTROL MEASURES ONCE UPSLOPE AREAS HAVE BEEN PERMANENTLY STABILIZED AND VEGETATED AREAS HAVE RECEIVED TWO MOWINGS.
- VEGETATIVE PRACTICE:

PERMANENT MEASURES:

 - SLOPES SHALL NOT BE STEEPER THAN 1 VERTICAL TO 3 HORIZONTAL UNLESS OTHERWISE SPECIFIED.
 - LOAM AND SEED REQUIREMENTS ARE SPECIFIED IN RIDOT L01 & L02.
 - A MINIMUM OF 4 INCHES OF LOAM SHALL BE INSTALLED. THE LOAM SHALL BE GRADED TO A SMOOTH CONDITION AND STONES AND OTHER OBJECTS LARGER THAN 2 INCHES SHALL BE REMOVED.

TEMPORARY MEASURES (FOR TEMPORARY PROTECTION OF DISTURBED AREAS)

 - LIMESTONE AND FERTILIZER SHALL BE APPLIED AT THE FOLLOWING RATE:
LIMESTONE: 3 TONS/ACRE
FERTILIZER: (10-10-10) 600 LBS/ACRE
 - SEED SHALL BE APPLIED AT THE FOLLOWING RATE:
WINTER RYE: 100 LBS/ACRE
 - STRAW MULCH SHALL BE APPLIED AT THE RATE OF 1.5 TONS/ACRE.
- MAINTENANCE

DURING THE PERIOD OF CONSTRUCTION AND/OR UNTIL LONG TERM VEGETATION IS ESTABLISHED, THE EROSION CONTROL MEASURES SHALL BE INSPECTED.

 - AT A MINIMUM THE SILT FENCING, STRAW BALES AND FILTER SOCK BARRIERS SHALL BE INSPECTED AND REPAIRED ONCE A WEEK AND / OR IMMEDIATELY FOLLOWING A SIGNIFICANT RAINFALL OR SNOWMELT. SEDIMENT TRAPPED BEHIND THE BARRIERS SHALL BE EXCAVATED WHEN IT REACHES A DEPTH OF 6" AND REGRADED ON THE SITE.
 - EROSION CONTROL BLANKETS SHALL BE INSPECTED ON A WEEKLY BASIS.
 - SILT SACKS SHALL BE INSPECTED AND REPAIRED ONCE A WEEK AND / OR IMMEDIATELY FOLLOWING A SIGNIFICANT RAINFALL OR SNOWMELT. DURING HEAVY RAIN EVENT, IT MAY BE NECESSARY TO TEMPORARILY REMOVE SACKS IN ORDER TO PREVENT FLOODING. SEDIMENT TRAPPED WITHIN SACKS SHALL BE DISPOSED OF OFF SITE AT A LICENSED FACILITY OR REGRADED ON THE SITE.
 - ANY STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSPECTED WEEKLY, AND REESTABLISHED AS NECESSARY.
 - SEEDDED AREAS WILL BE FERTILIZED AND RESEDED AS NECESSARY TO INSURE ESTABLISHMENT OF A VEGETATIVE GROWTH THAT MEETS THE APPROVAL OF THE CITY ENGINEER.

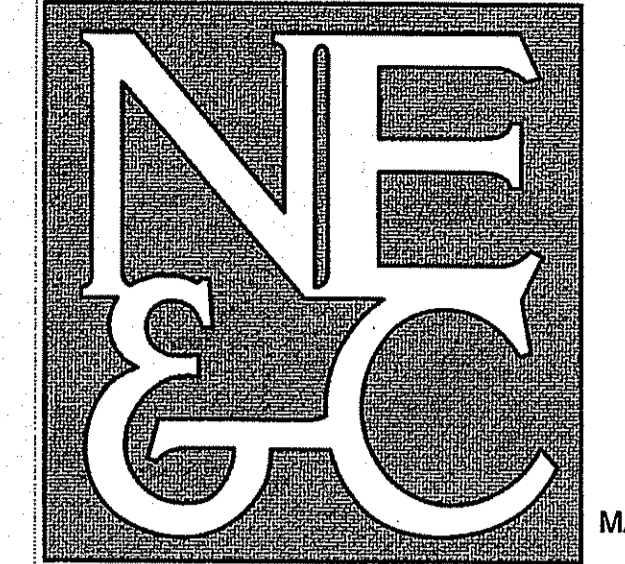
ABBREVIATIONS

AC	ASPHALT PAVEMENT
ADJ	ADJACENT
AP	ASSESSOR'S PLAT
ACCESS	ACCESSIBLE
ARCH	ARCHITECT
BC	BOTTOM OF CURB
BH	BORING HOLE
BIT	BITUMINOUS
CLR	CLEARANCE
CMP	CORRUGATED METAL PIPE
CMU	CONCRETE MASONRY UNIT
CNC	CONCRETE
CONN	CONNECT
COTG	CLEAN OUT TO GRADE
CF	CUBIC FOOT
DEMO	DEMOLISH
DI	DRAIN INLET
DIA	DIAMETER
DMH	DRAIN MANHOLE
DP	DROP PIPE
DS	DOWN SPOUT
DWS	DRAINING
E	ELECTRIC
ELEV	ELEVATION
EMH	ELECTRIC MANHOLE
EOP	EDGE OF PAVEMENT
EQ	EQUAL
EXP	EXISTING
EXP	EXPANSION
FFE	FINISH FLOOR ELEVATION
FG	FINISHED GRADE
FH	FIRE HYDRANT
FT	FEET
G	GAS
GAL	GALLON
GV	GATE VALVE
INV	INVERT
JT	JOINT
L	LENGTH
LAND	LANDSCAPE
NTS	NOT TO SCALE
MAX	MAXIMUM
MECH	MECHANICAL
MIN	MINIMUM
PAVT	PAVEMENT
PVC	POLYVINYLCHLORIDE
R	RADIUS
RC	REINFORCED CONCRETE
RECONN	RECONNECT
RIB	RHODE ISLAND HIGHWAY BOUND
RIDOT	RHODE ISLAND DEPARTMENT OF TRANSPORTATION
ROW	RIGHT OF WAY
S	SLOPE SEWER
SF	SQUARE FEET
SHT	SHEET
SMH	SEWER MANHOLE
STA	STATION
STRUCT	STRUCTURAL
TC	TOP OF CURB
TOP	TOP OF SURFACE
UGT	UNDERGROUND TELEPHONE
TYP	TYPICAL
W	WATER
W	WITH
WWM	WELDED WIRE MESH

LEGEND

---	PROPERTY LINE
---	ABUTTER PROPERTY LINE
---	PROPOSED SUBDIVISION LINE
---	ZONING SETBACK LINE
---	45' TOPOGRAPHIC CONTOUR
X	CHAIN LINK FENCE
○	RAIL FENCE
---	EXISTING PAVEMENT
---	TREE LINE
---	EXISTING SOILS
---	EXISTING DRAIN LINE
---	EXISTING SEWER LINE
---	EXISTING WATER LINE
---	EXISTING GAS LINE
---	PROPOSED SEWER MAIN
---	PROPOSED SEWER SERVICE
---	PROPOSED ELECTRIC SERVICE
---	PROPOSED WATER SERVICE
---	PROPOSED GAS SERVICE
---	PROPOSED GRADING
○	DRAIN MANHOLE
○	SEWER MANHOLE
○	TELEPHONE MANHOLE
○	MANHOLE
⊗	CATCH BASIN
⊗	HYDRANT
⊗	UTILITY POLE
⊗	WATER GATE
⊗	GAS GATE
⊗	PROPOSED GAS GATE
⊗	PROPOSED WATER GATE
⊗	DELINEATED WETLAND
⊗	WETLAND FLAG
⊗	STONE/GRANITE BOUND
○	IRON PIPE
---	PROPOSED UTILITY EASEMENT
---	UTILITY & CURB CUT LIMIT OF DISTURBANCE
---	OVERALL LIMIT OF DISTURBANCE
---	SILT FENCE & STRAW BALES
---	SILT FENCE
⊗	4" x 4" WOODEN POST

NORTHEAST ENGINEERS & CONSULTANTS, INC.



A KNOWLEDGE CORPORATION

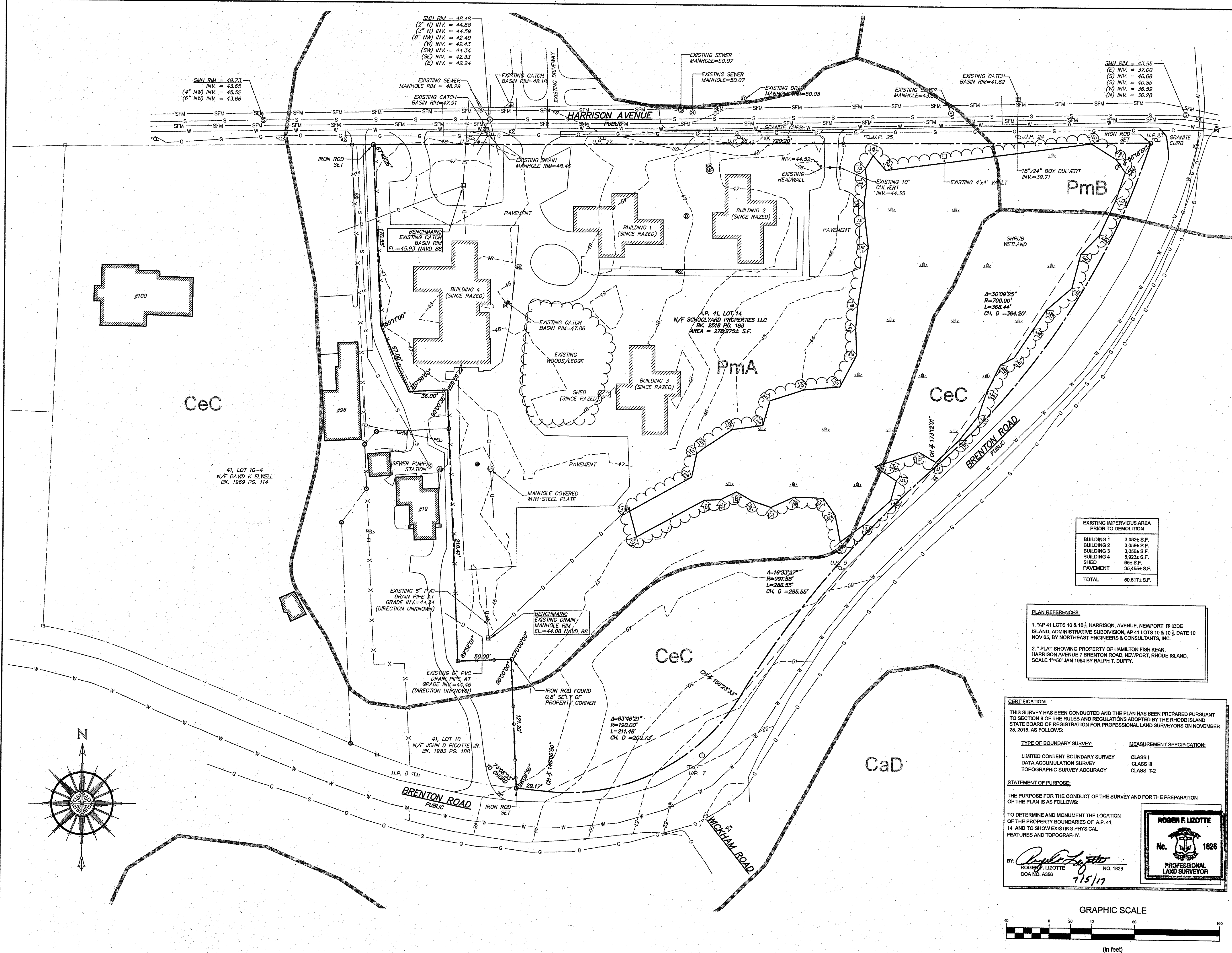
55 JOHN CLARKE ROAD MIDDLETOWN RHODE ISLAND 02842
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WWW.NORTHEASTENGINEERS.COM

- SITE/CIVIL
- LAND PLANNING
- WATERFRONT
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- ENVIRONMENTAL
- TRANSPORTATION
- STRUCTURAL
- MATERIALS TESTING

DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO.: 17-063
DATED AUG 24, 2017
SEE LETTER OF SAME DATE.

Martin D. Wencel

No.	Revision	Date	App.
Designed By: JJR	Drawn by: DJW	Checked by:	
Scale: N.T.S.	Date: 13APR17		
Project Title:			
<p>A.P. 41, LOT 14 90 HARRISON AVENUE FORMER UNDERWOOD SCHOOL NEWPORT, RHODE ISLAND</p>			
Client/Owner:			
<p>SCHOOLYARD PROPERTIES LLC 19 BRENTON ROAD NEWPORT, RI 02840</p>			
Issued for:			
RIDEM			
Drawing Title:			
NOTES			
Drawing Number:		N-1	
Sheet 2 of 7		Project Number:	
		16068.1	
Survey Index:		14 - 41 - 14	
<p>OWNERSHIP AND USE OF DOCUMENTS: DRAWINGS AND SPECIFICATIONS, AS INSTRUMENTS OF PROFESSIONAL SERVICE, ARE AND SHALL REMAIN THE PROPERTY OF THE ENGINEER. THESE DOCUMENTS ARE NOT TO BE USED, IN WHOLE OR PART, FOR ANY OTHER PROJECTS OR PURPOSES, OR BY ANY OTHER PARTIES, THAN THOSE PROPERLY AUTHORIZED BY CONTRACT, WITHOUT THE EXPRESS AUTHORIZATION OF THE ENGINEER.</p>			



SMH RIM = 48.48
 (2" N) INV. = 44.88
 (3" N) INV. = 44.39
 (6" NW) INV. = 42.49
 (W) INV. = 42.43
 (SW) INV. = 44.34
 (SE) INV. = 42.33
 (E) INV. = 42.24

SMH RIM = 43.55
 (E) INV. = 37.00
 (S) INV. = 40.68
 (W) INV. = 36.59
 (N) INV. = 39.28

EXISTING IMPERVIOUS AREA PRIOR TO DEMOLITION	
BUILDING 1	3,052± S.F.
BUILDING 2	3,056± S.F.
BUILDING 3	3,056± S.F.
BUILDING 4	5,923± S.F.
SHED	95± S.F.
PAVEMENT	35,455± S.F.
TOTAL	50,617± S.F.

- PLAN REFERENCES:**
- "AP 41 LOTS 10 & 14, HARRISON AVENUE, NEWPORT, RHODE ISLAND, ADMINISTRATIVE SUBDIVISION, AP 41 LOTS 10 & 14, DATE 10 NOV 05, BY NORTHEAST ENGINEERS & CONSULTANTS, INC.
 - "PLAT SHOWING PROPERTY OF HAMILTON FISH KEAN, HARRISON AVENUE 7 BRENTON ROAD, NEWPORT, RHODE ISLAND, SCALE 1"=50' JAN 1954 BY RALPH T. DUFFY.

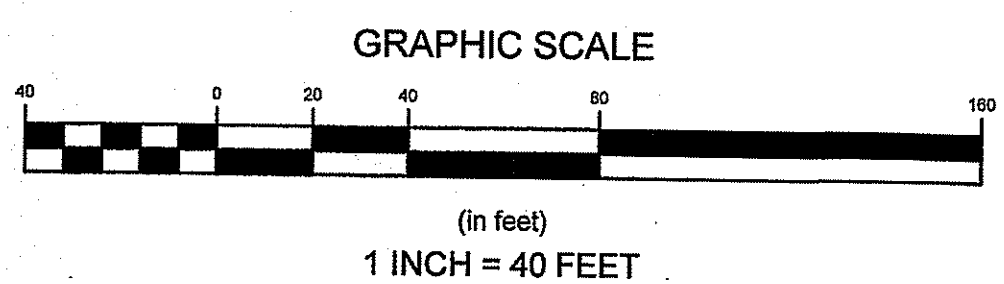
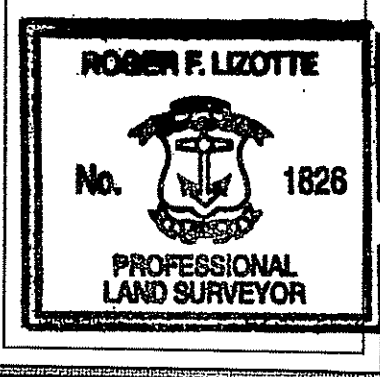
CERTIFICATION:
 THIS SURVEY HAS BEEN CONDUCTED AND THE PLAN HAS BEEN PREPARED PURSUANT TO SECTION 9 OF THE RULES AND REGULATIONS ADOPTED BY THE RHODE ISLAND STATE BOARD OF REGISTRATION FOR PROFESSIONAL LAND SURVEYORS ON NOVEMBER 25, 2015, AS FOLLOWS:

TYPE OF BOUNDARY SURVEY:	MEASUREMENT SPECIFICATION:
LIMITED CONTENT BOUNDARY SURVEY	CLASS I
DATA ACCUMULATION SURVEY	CLASS III
TOPOGRAPHIC SURVEY ACCURACY	CLASS T-2

STATEMENT OF PURPOSE:
 THE PURPOSE FOR THE CONDUCT OF THE SURVEY AND FOR THE PREPARATION OF THE PLAN IS AS FOLLOWS:

TO DETERMINE AND MONUMENT THE LOCATION OF THE PROPERTY BOUNDARIES OF A.P. 41, 14 AND TO SHOW EXISTING PHYSICAL FEATURES AND TOPOGRAPHY.

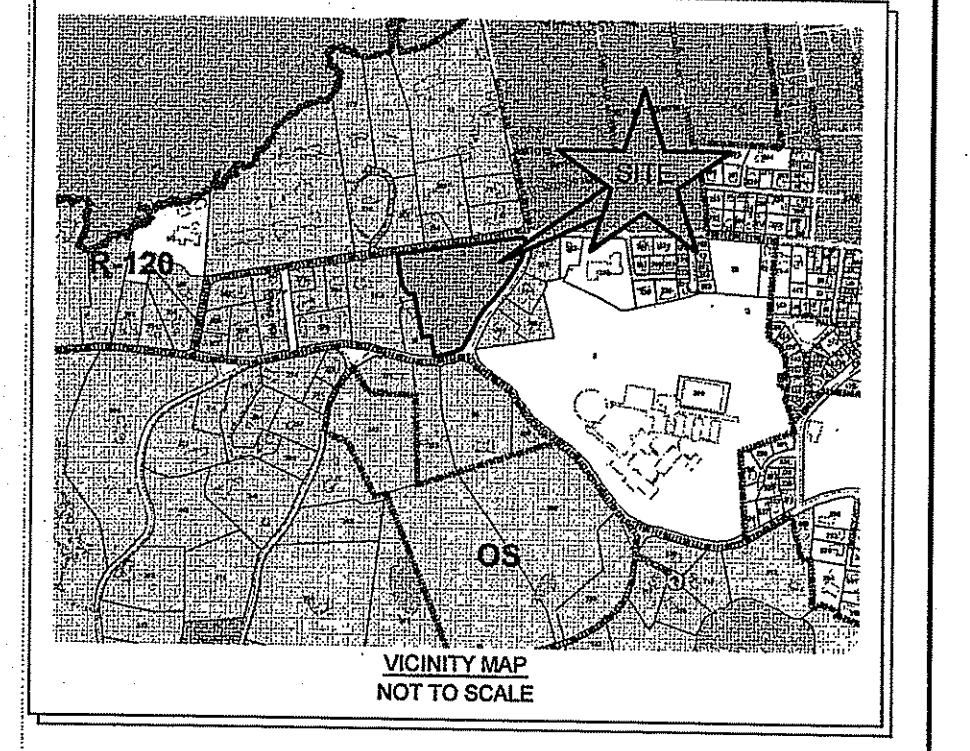
BY: *Roger F. Lizotte*
 ROGER F. LIZOTTE
 COA NO. A355
 7/15/17



NORTHEAST ENGINEERS & CONSULTANTS, INC.

A KNOWLEDGE CORPORATION

55 JOHN CLARKE ROAD MIDDLETOWN RHODE ISLAND 02842
 PHONE (401) 849-0810 FAX (401) 846-4169
 WWW.NORTHEASTENGINEERS.COM

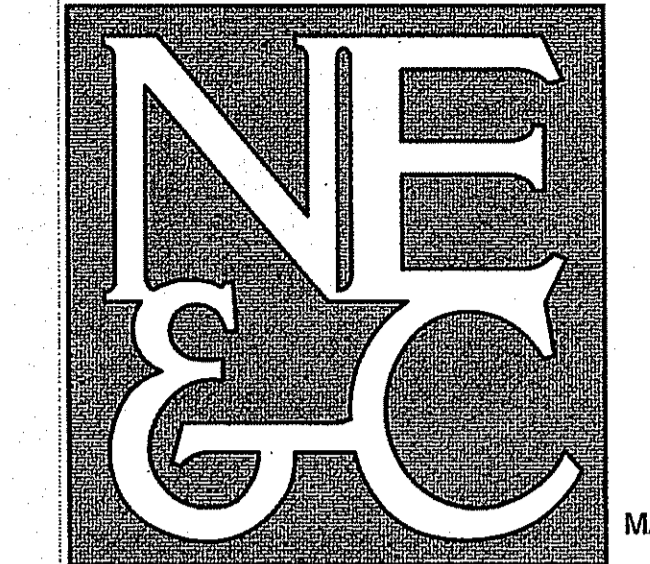


DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
 OFFICE OF WATER RESOURCES
 FRESHWATER WETLANDS PROGRAM
 REVIEWED SITE PLAN APPLICATION NO.: 17-0163
 DATED AUG 7, 2017
 SEE LETTER OF SAME DATE.

Martin B. Wemen

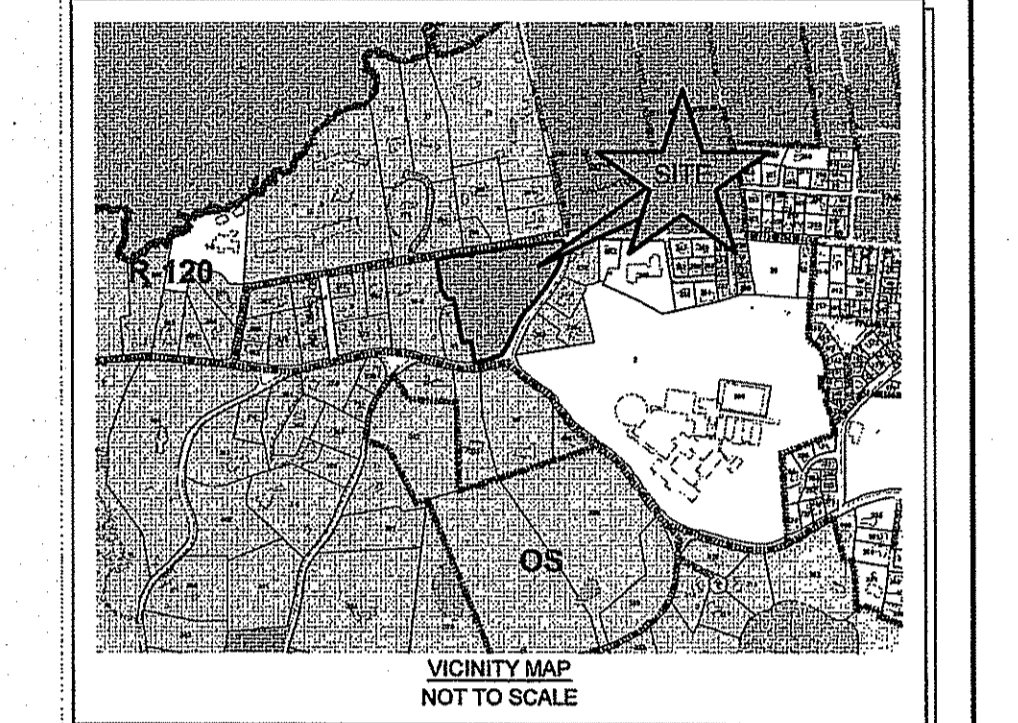
No.	Revision	Date	App.
Designed By:	Drawn by: DJW	Checked by: RFL	
Scale:	1" = 40'	Date:	13APR17
Project Title:			
A.P. 41, LOT 14			
90 HARRISON AVENUE			
FORMER UNDERWOOD SCHOOL NEWPORT, RHODE ISLAND			
Client/Owner:			
SCHOOLYARD PROPERTIES LLC 19 BRENTON ROAD NEWPORT, RI 02840			
Issued for:			
RIDEM			
Drawing Title:			
EXISTING CONDITIONS PLAN			
Drawing Number:			
EX-1			
Sheet 3 of 7			
Project Number:			
16068.1			
Survey Index:			
14 - 41 - 14			

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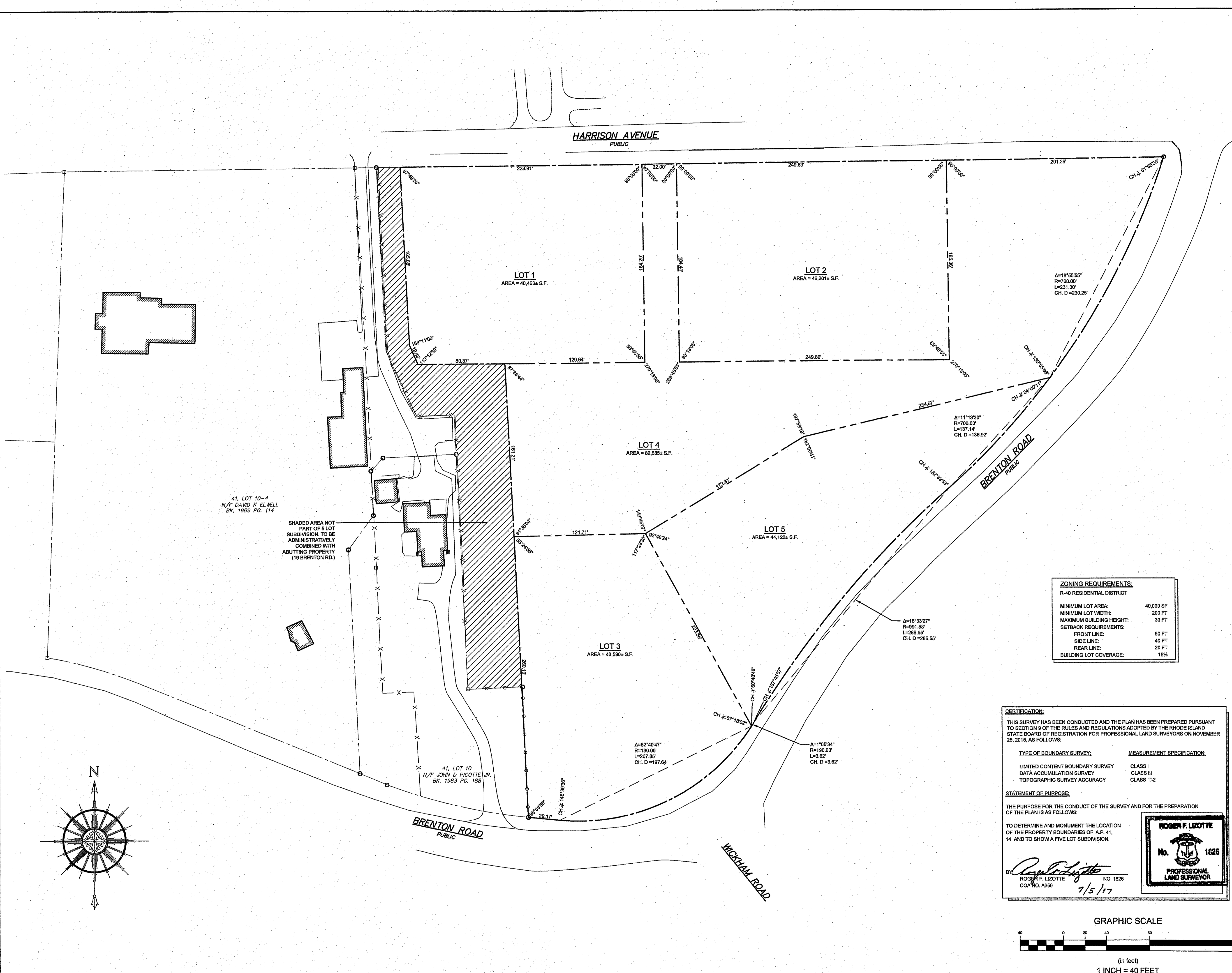
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DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO.: 17-0163
DATED AUG 24 2017
SEE LETTER OF SAME DATE.
Martin D. Jensen

No.	Revision	Date	App.
Designed By:	JJR	Drawn by:	DJW
Checked by:	RFL	Date:	13APR17
Project Title:			
A.P. 41, LOT 14 90 HARRISON AVENUE FORMER UNDERWOOD SCHOOL NEWPORT, RHODE ISLAND			
Client/Owner:			
SCHOOLYARD PROPERTIES LLC 19 BRENTON ROAD NEWPORT, RI 02840			
Issued for:			
RIDEM			
Drawing Title:			
PROPOSED SUBDIVISION PLAN			
Drawing Number:			
SUB-1			
Sheet:			
4 of 7			
Project Number:			
16068.1			
Survey Index:			
14 - 41 - 14			
OWNERSHIP AND USE OF DOCUMENTS: DRAWINGS AND SPECIFICATIONS, AS INSTRUMENTS OF PROFESSIONAL SERVICE, ARE AND SHALL REMAIN THE PROPERTY OF THE ENGINEER. THESE DOCUMENTS ARE NOT TO BE USED, IN WHOLE OR PART, FOR ANY OTHER PROJECTS OR PURPOSES, OR BY ANY OTHER PARTIES, THAN THOSE PROPERLY AUTHORIZED BY CONTRACT, WITHOUT THE EXPRESS AUTHORIZATION OF THE ENGINEER.			



ZONING REQUIREMENTS:
R-40 RESIDENTIAL DISTRICT

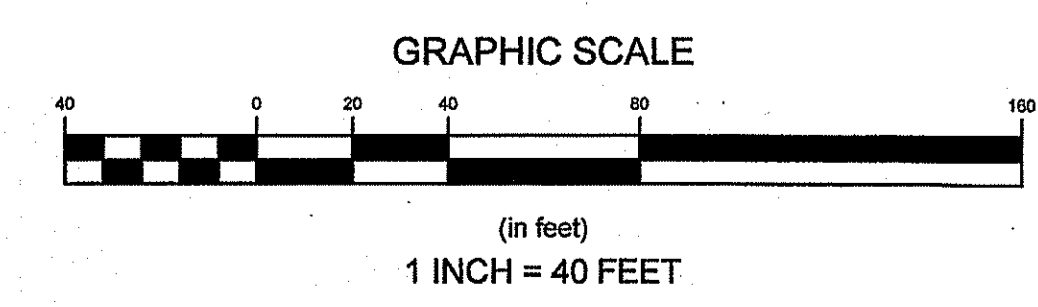
MINIMUM LOT AREA:	40,000 SF
MINIMUM LOT WIDTH:	200 FT
MAXIMUM BUILDING HEIGHT:	30 FT
SETBACK REQUIREMENTS:	
FRONT LINE:	50 FT
SIDE LINE:	40 FT
REAR LINE:	20 FT
BUILDING LOT COVERAGE:	15%

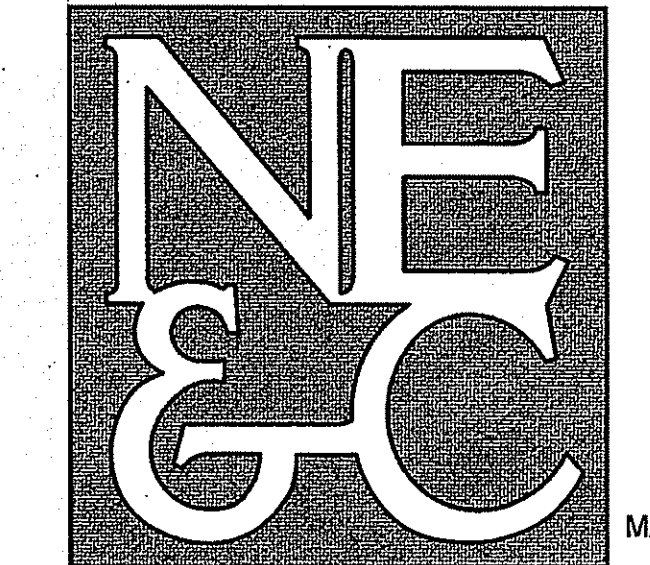
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STATEMENT OF PURPOSE:
THE PURPOSE FOR THE CONDUCT OF THE SURVEY AND FOR THE PREPARATION OF THE PLAN IS AS FOLLOWS:
TO DETERMINE AND MONUMENT THE LOCATION OF THE PROPERTY BOUNDARIES OF A.P. 41, 14 AND TO SHOW A FIVE LOT SUBDIVISION.

BY: *Roger F. Lizotte*
ROGER F. LIZOTTE NO. 1826
COA NO. A358 7/5/17

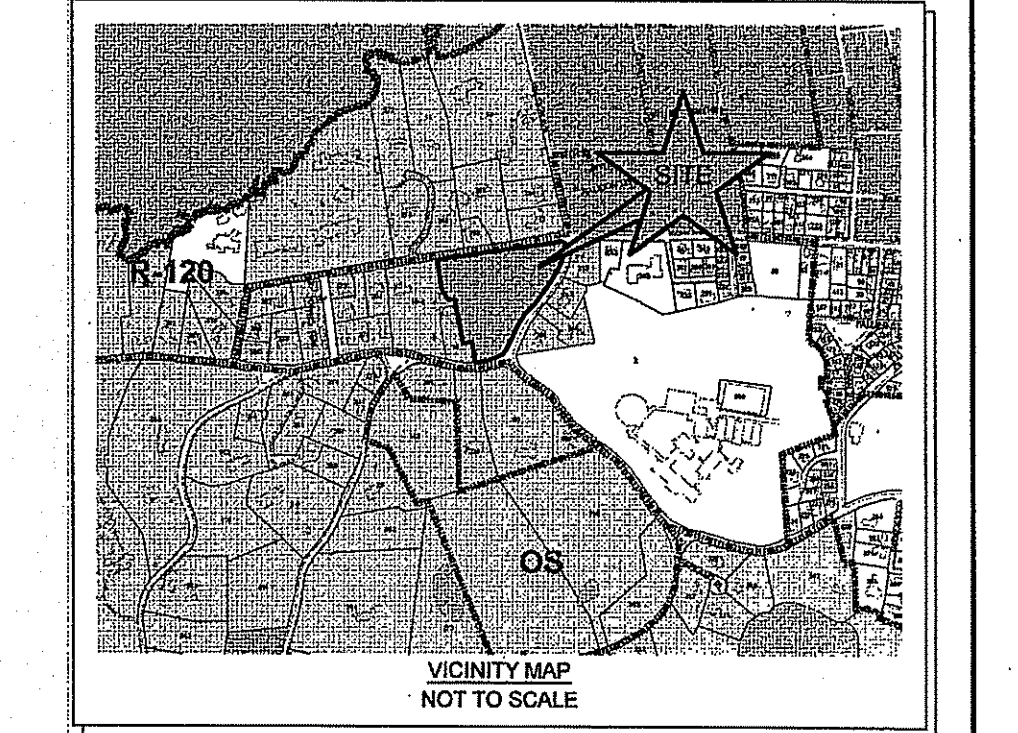




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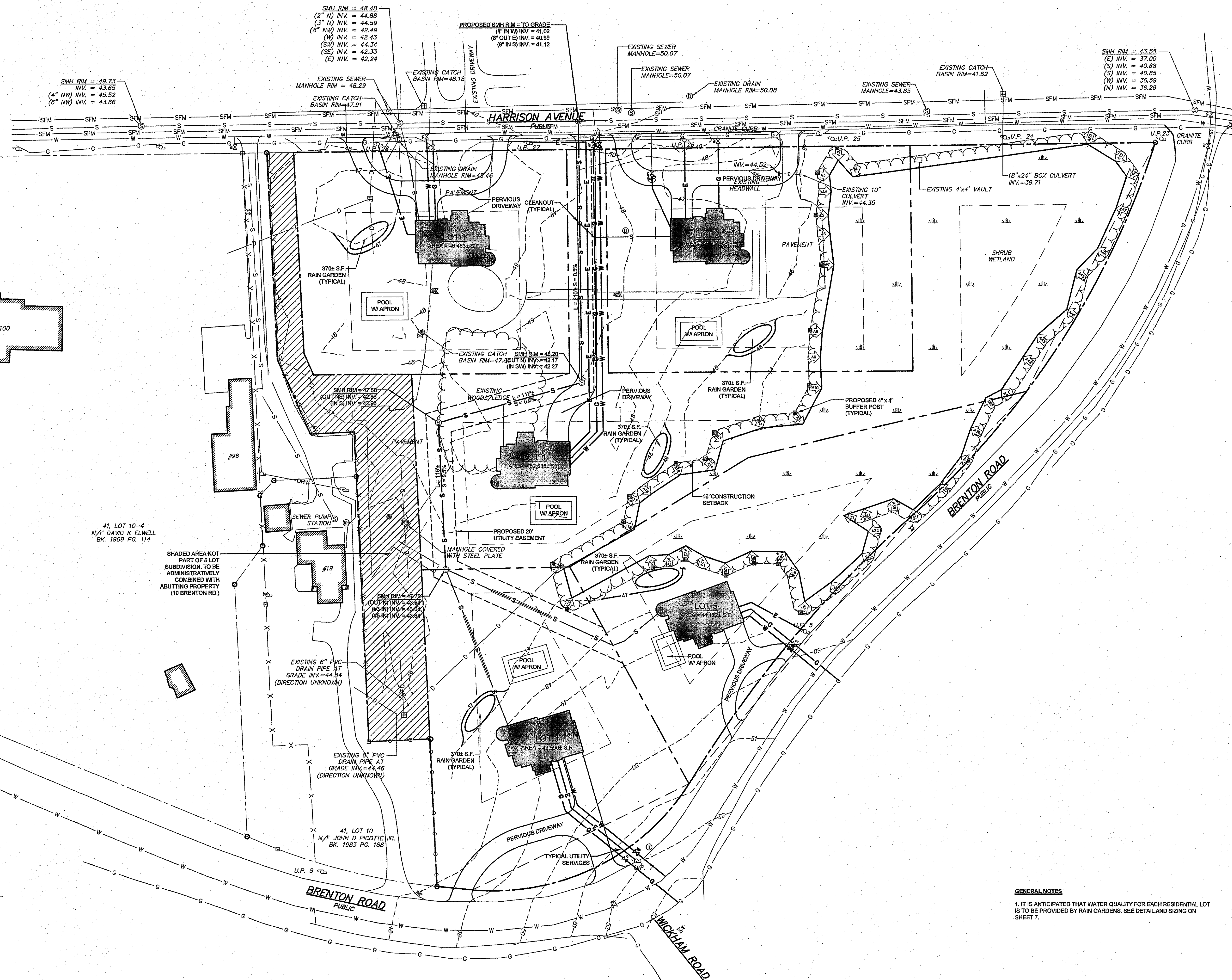
55 JOHN CLARKE ROAD MIDDLETOWN RHODE ISLAND 02842
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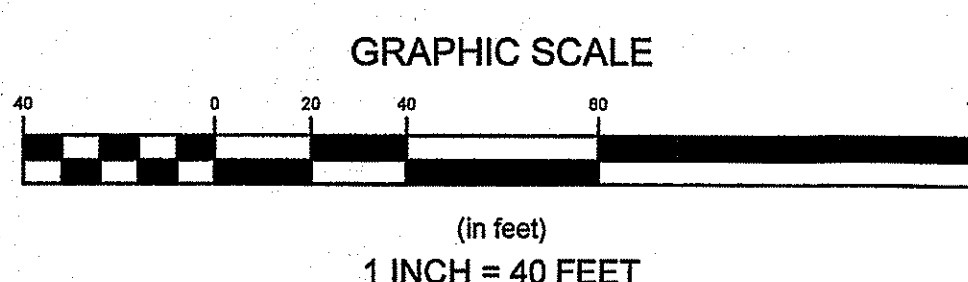
DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO.: 17-0163
DATED AUG 24, 2011
SEE LETTER OF SAME DATE.

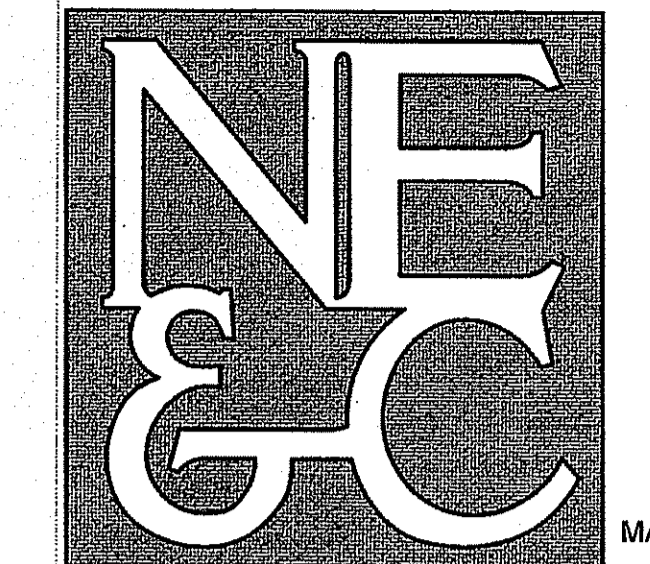
Martin D. Womack

No.	Revision	Date	App.
Designed By: JJR	Drawn by: DJW	Checked by:	
Scale: 1" = 40'	Date: 13APR17		
Project Title: A.P. 41, LOT 14 90 HARRISON AVENUE FORMER UNDERWOOD SCHOOL NEWPORT, RHODE ISLAND			
Client/Owner: SCHOOLYARD PROPERTIES LLC 19 BRENTON ROAD NEWPORT, RI 02840			
Issued for: RIDEM			
Drawing Title: PROPOSED UTILITY & DRAINAGE PLAN			
Drawing Number: UTL-1		Sheet 5 of 7	
Project Number: 16068.1		Survey Index: 14 - 41 - 14	
<p>OWNERSHIP AND USE OF DOCUMENTS: DRAWINGS AND SPECIFICATIONS, AS INSTRUMENTS OF PROFESSIONAL SERVICE, ARE AND SHALL REMAIN THE PROPERTY OF THE ENGINEER. THESE DOCUMENTS ARE NOT TO BE USED, IN WHOLE OR PART, FOR ANY OTHER PROJECTS OR PURPOSES, OR BY ANY OTHER PARTIES, THAN THOSE PROPERLY AUTHORIZED BY CONTRACT, WITHOUT THE EXPRESS AUTHORIZATION OF THE ENGINEER.</p>			



GENERAL NOTES
1. IT IS ANTICIPATED THAT WATER QUALITY FOR EACH RESIDENTIAL LOT IS TO BE PROVIDED BY RAIN GARDENS. SEE DETAIL AND SIZING ON SHEET 7.

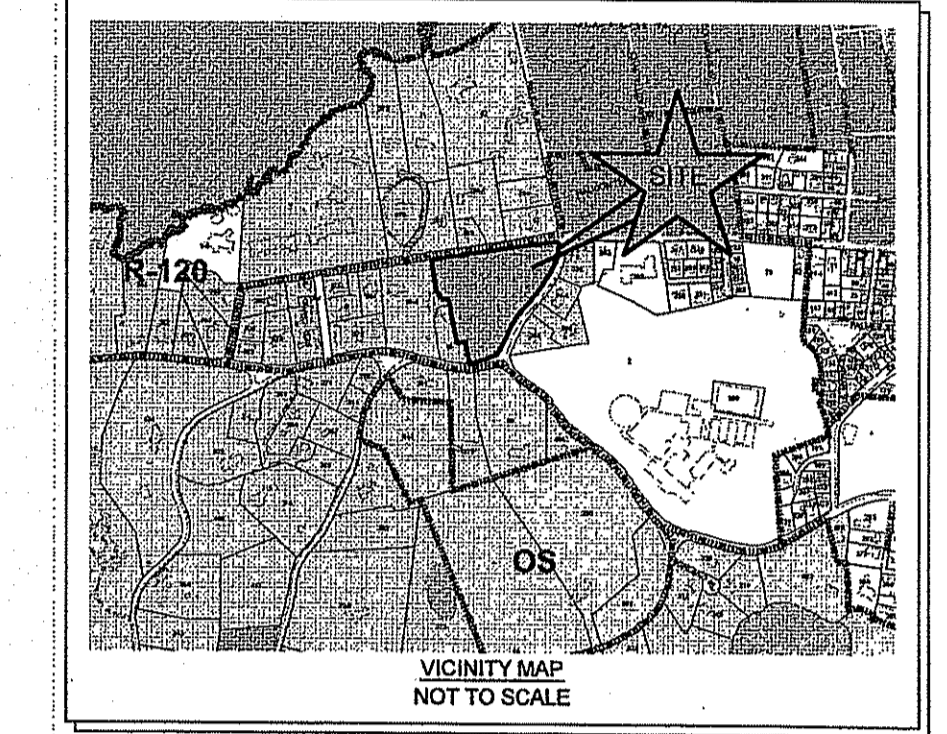




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VICINITY MAP
NOT TO SCALE

DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO.: 17-0163
DATED JUN 24 2011
SEE LETTER OF SAME DATE.

Martin D. Wenczek

No.	Revision	Date	App.

Designed by: JJR Drawn by: DJW Checked by:
Scale: 1" = 40' Date: 13APR17

Project Title:
A.P. 41, LOT 14
80 HARRISON AVENUE
FORMER UNDERWOOD SCHOOL
NEWPORT, RHODE ISLAND

Client/Owner:
SCHOOLYARD PROPERTIES LLC
19 BRENTON ROAD
NEWPORT, RI 02840

Issued for:
RIDEM

Drawing Title:
**CONSTRUCTION
GRADING AND
SESC**

Drawing Number:
SESC-1

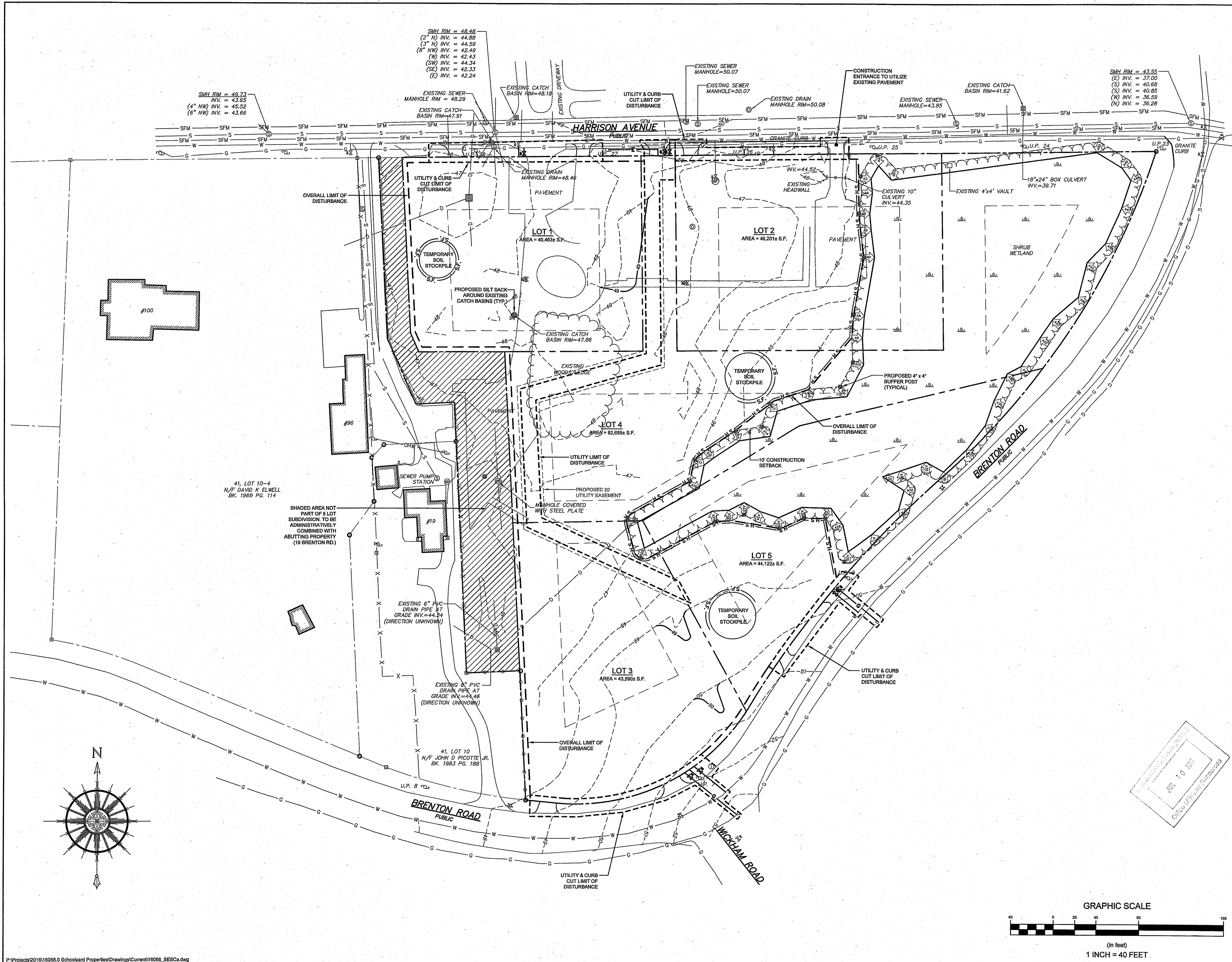
Sheet 6 of 7

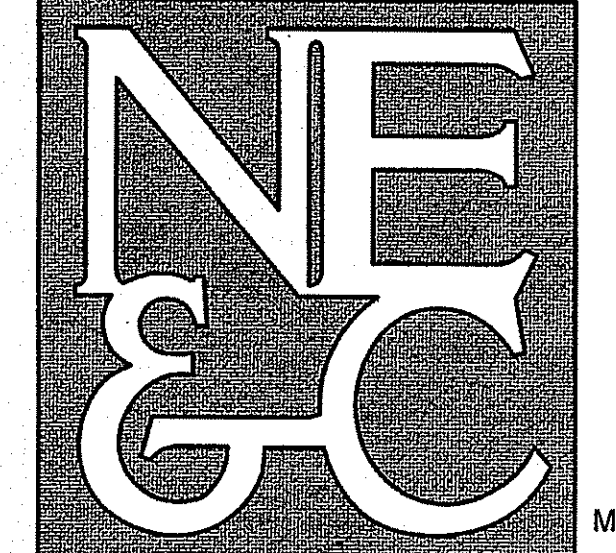
Project Number:
16068.1

Survey Index:
14 - 41 - 14

GERALD E. SMALL
No. **8550**
**REGISTERED
PROFESSIONAL ENGINEER
CIVIL**

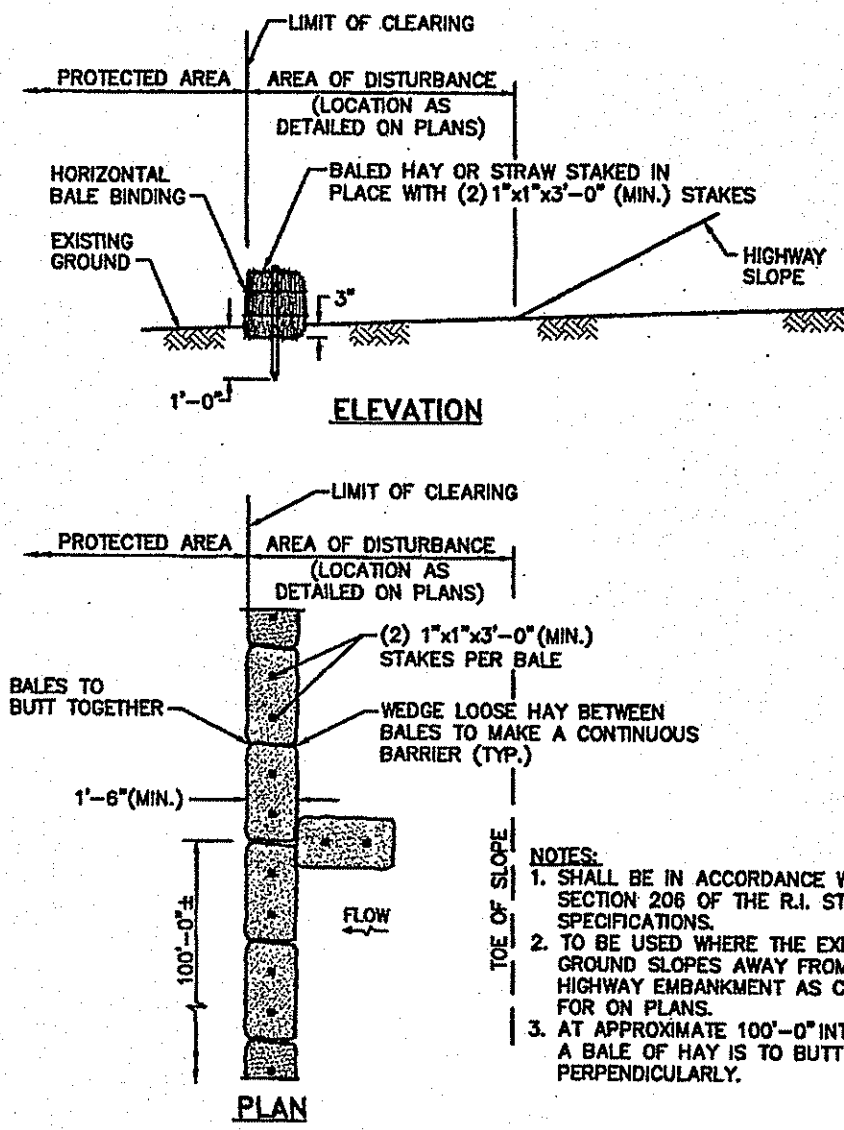
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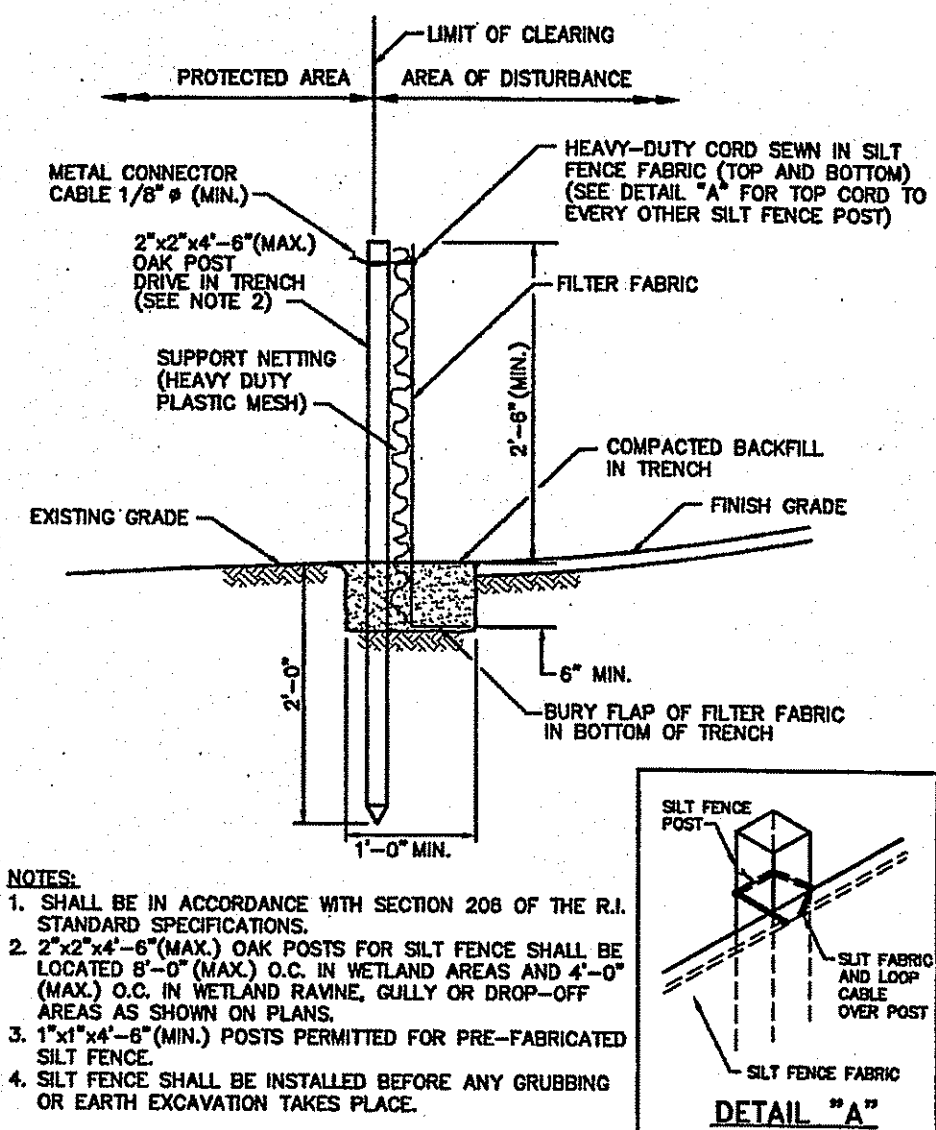


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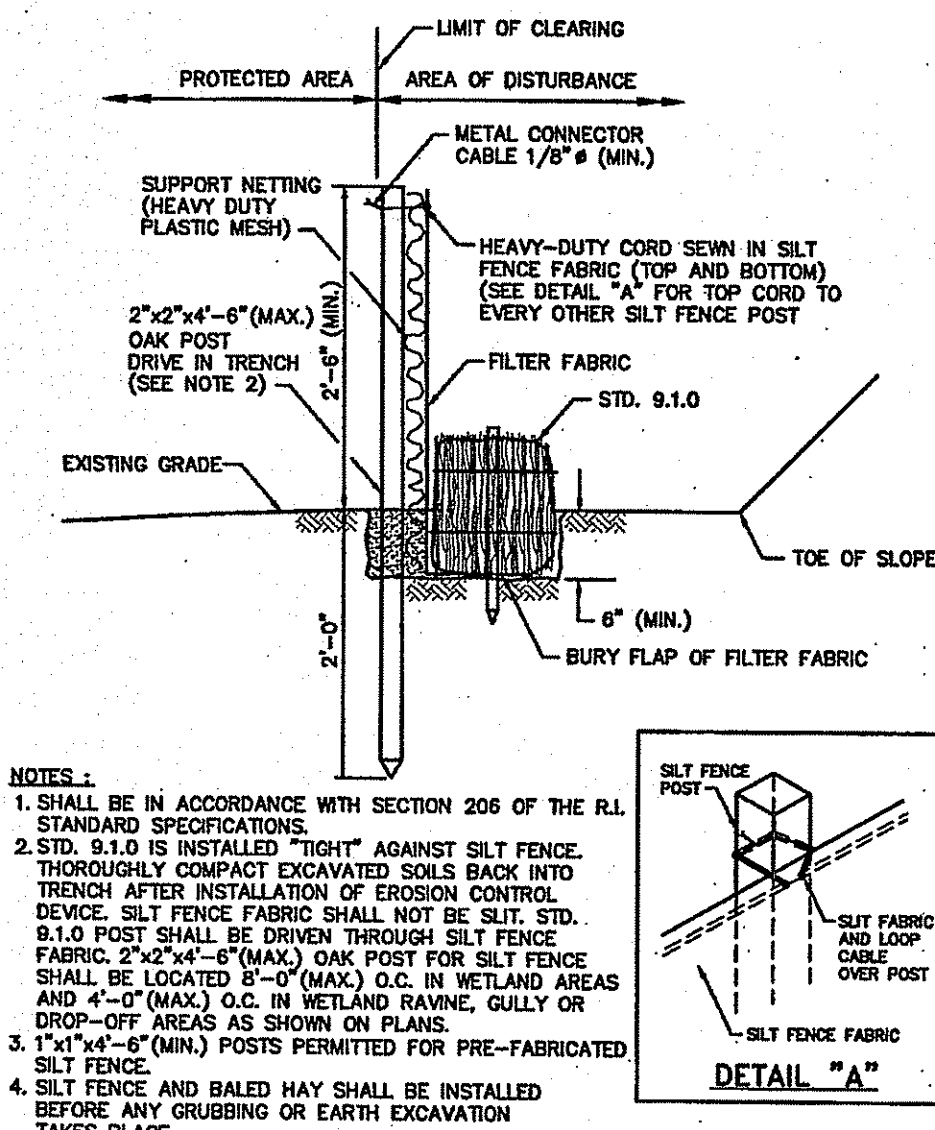
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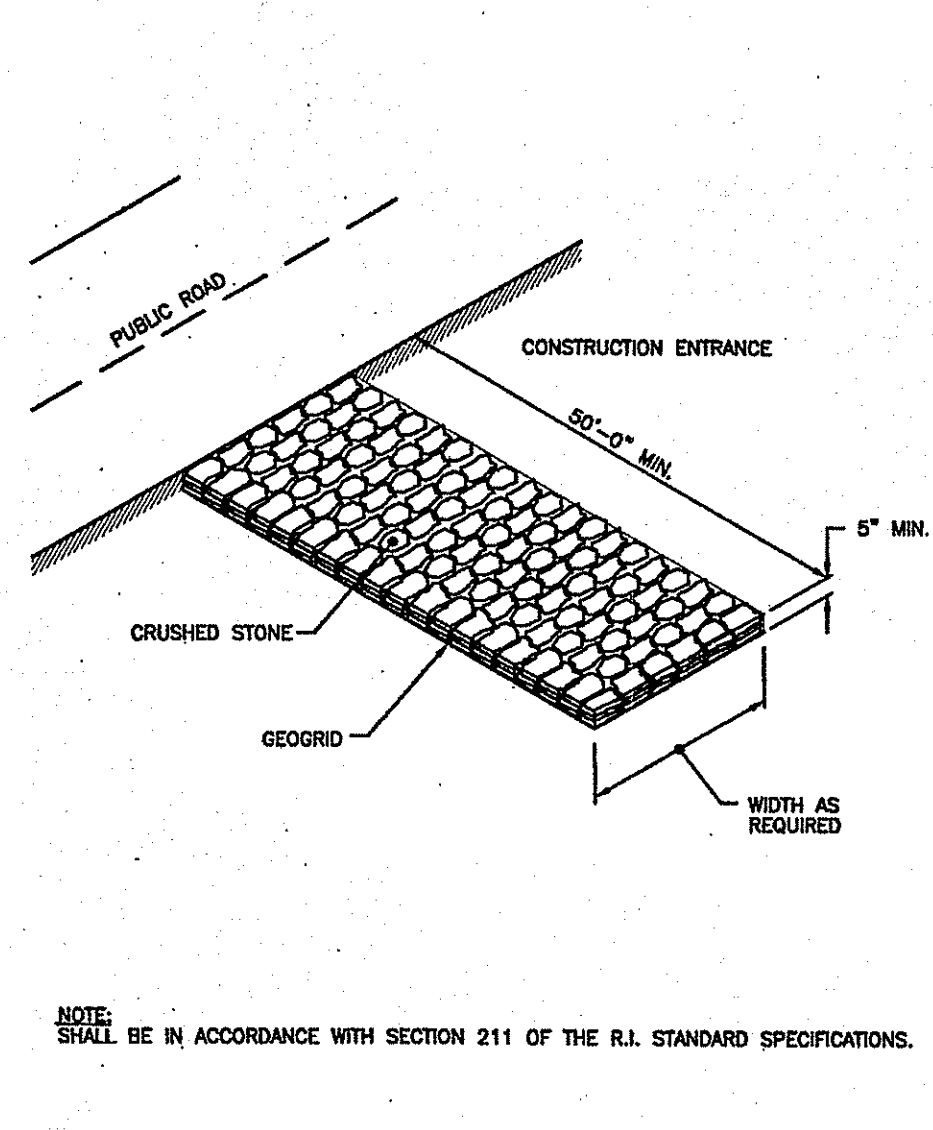
BALED STRAW EROSION CHECK (RIDOT 9.1.0)
SCALE: NOT TO SCALE



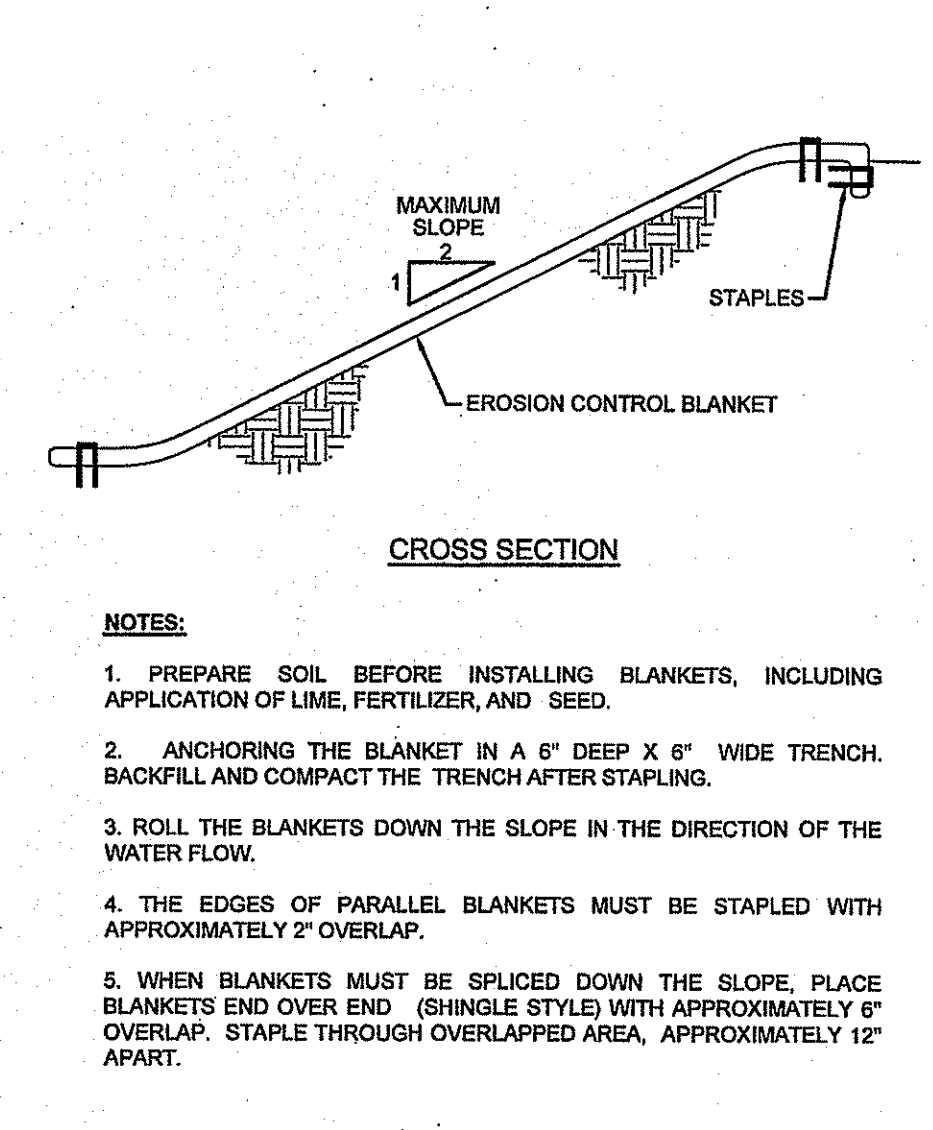
SILT FENCE EROSION CHECK (RIDOT 9.2.0)
SCALE: NOT TO SCALE



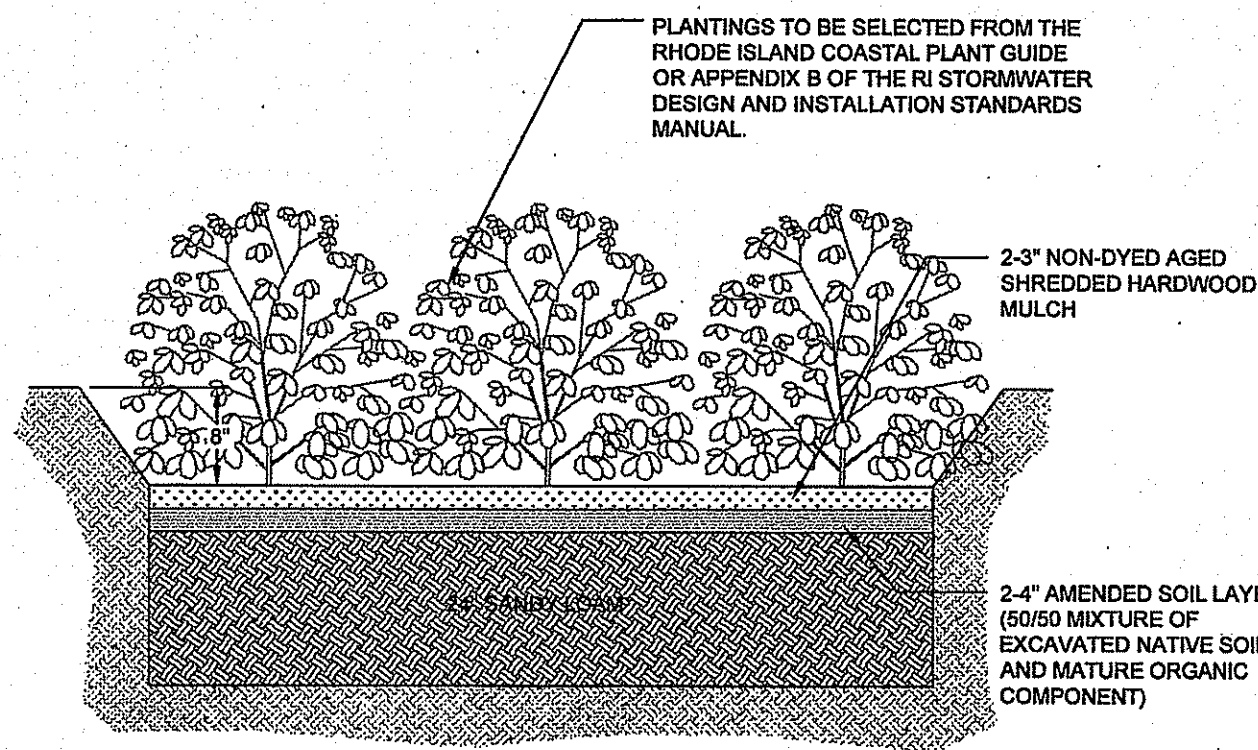
BALED STRAW EROSION CHECK AND SILT FENCE COMBINED (RIDOT 9.3.0)
SCALE: NOT TO SCALE



STONE CONSTRUCTION ACCESS (RIDOT 9.9.0)
SCALE: NOT TO SCALE



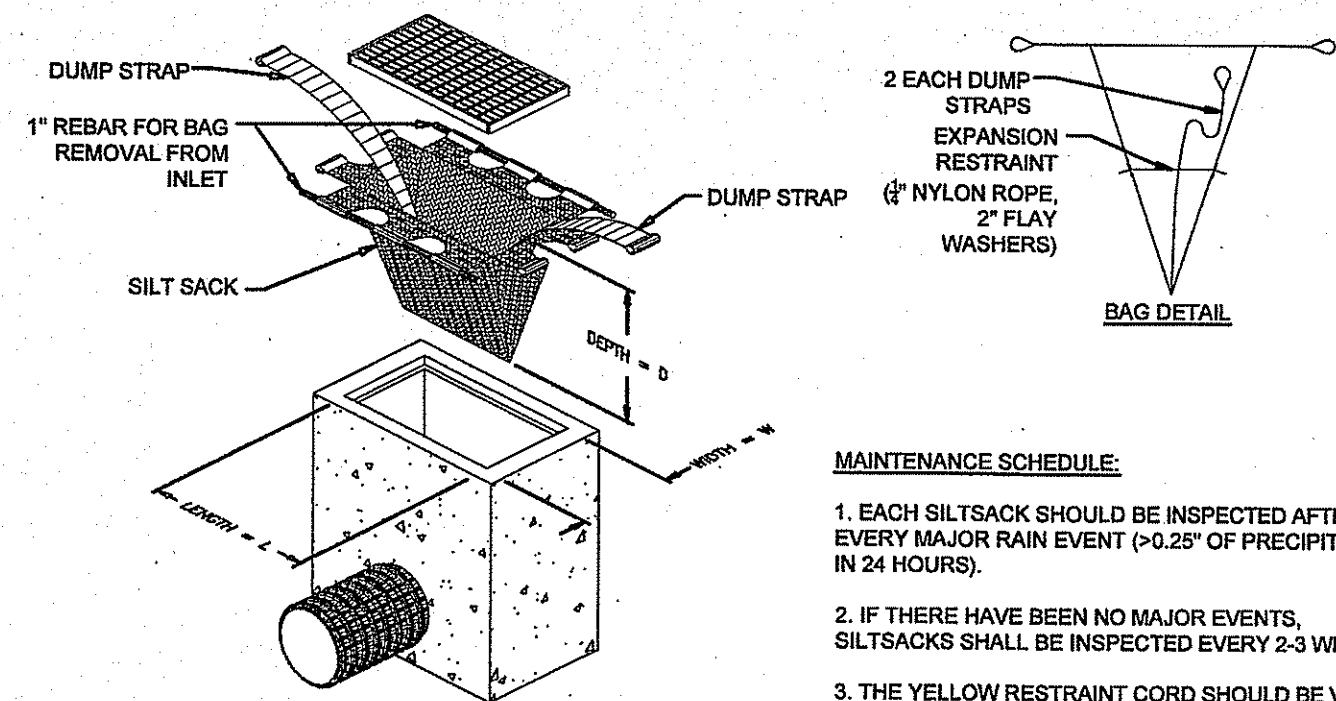
EROSION CONTROL BLANKET
SCALE: NOT TO SCALE



TYPICAL RAIN GARDEN CROSS SECTION
SCALE: NOT TO SCALE

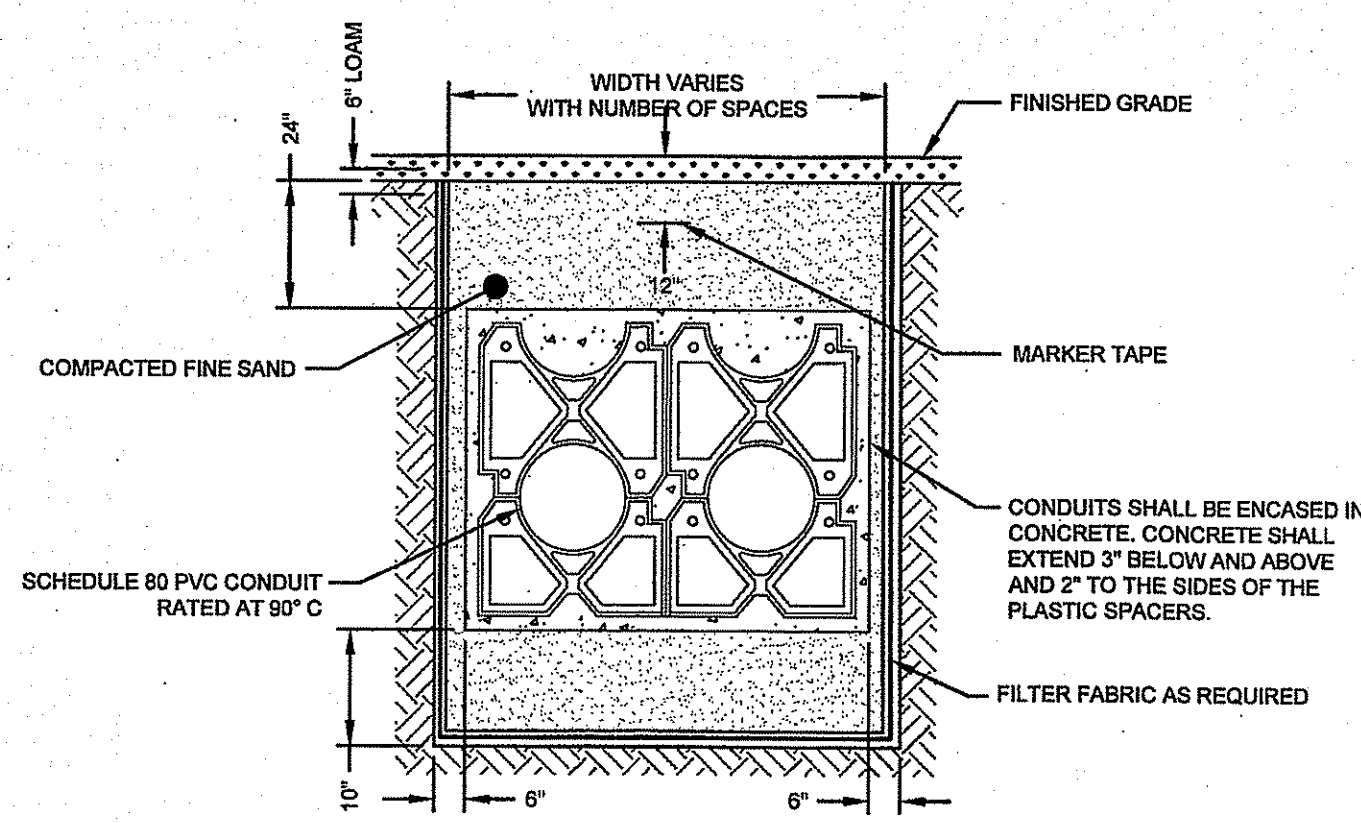
TOTAL IMPERVIOUS LOT AREA (SF)	GARDEN AREA REQUIRED (SF)
1,000	160
2,000	320
3,000	480
4,000	640
5,000	800
6,000	960
7,000	1,120
8,000	1,280
9,000	1,440
10,000	1,600

*BASED ON 160 SQ. FT. OF 8 INCH DEEP RAIN GARDEN PER 1,000 SQ. FT. OF CONTRIBUTING IMPERVIOUS AREA



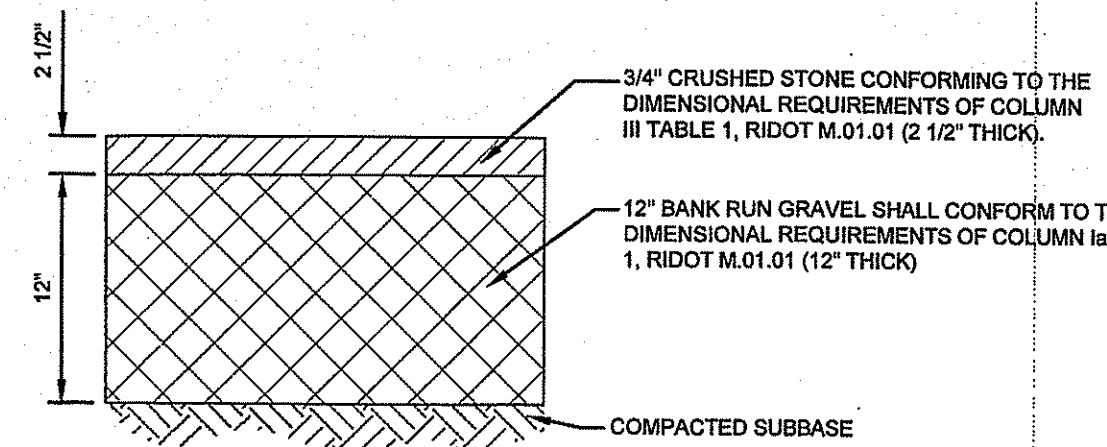
SILT SACK DETAIL
SCALE: NOT TO SCALE

- MAINTENANCE SCHEDULE:**
- EACH SILT SACK SHOULD BE INSPECTED AFTER EVERY MAJOR RAIN EVENT (>0.25" OF PRECIPITATION IN 24 HOURS).
 - IF THERE HAVE BEEN NO MAJOR EVENTS, SILT SACKS SHALL BE INSPECTED EVERY 2-3 WEEKS.
 - THE YELLOW RESTRAINT CORD SHOULD BE VISIBLE AT ALL TIMES. IF THE CORD IS COVERED WITH SEDIMENT, THE SILT SACK SHOULD BE EMPTIED.

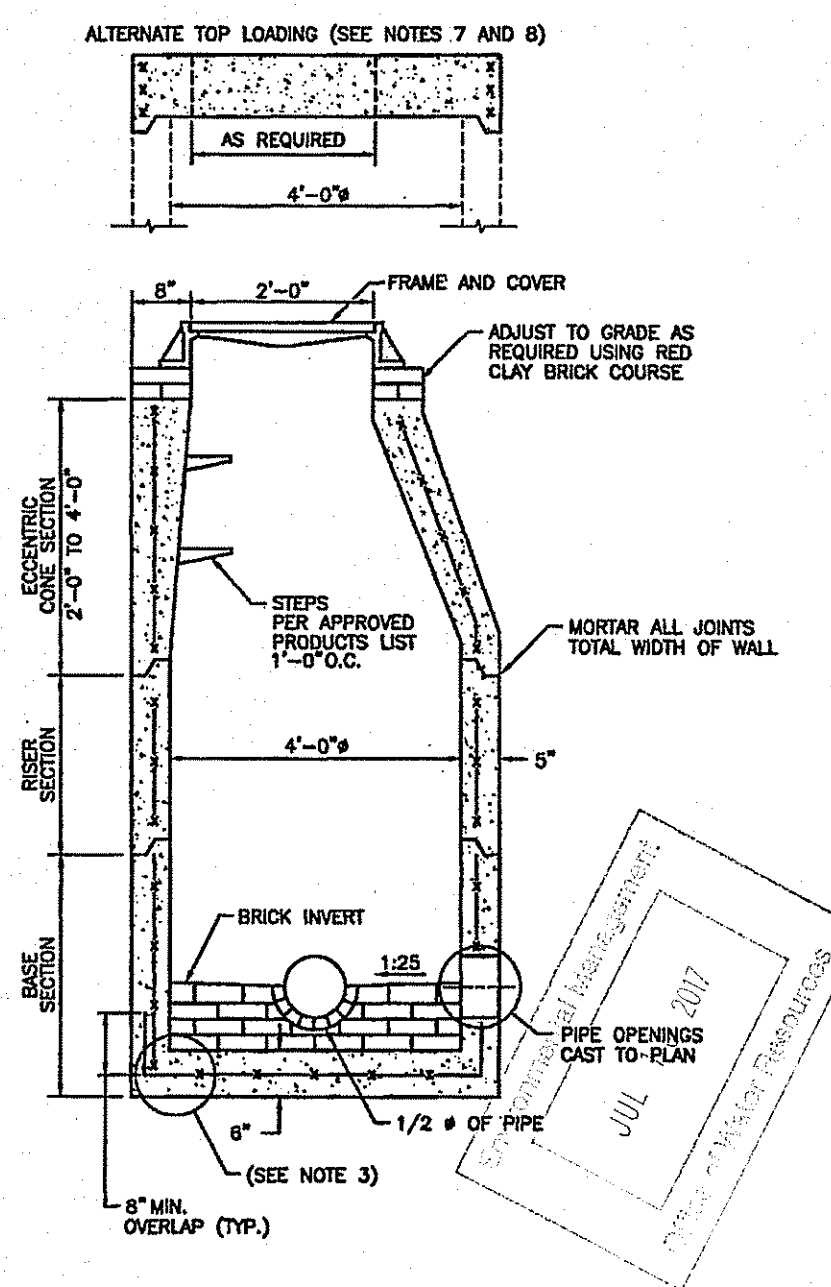


CONDUIT TRENCH CROSS SECTION
SCALE: NOT TO SCALE

- TRENCH NOTES:**
- UNSUITABLE MATERIAL SHALL BE EXCAVATED TO A MINIMUM DEPTH OF 12-INCHES BELOW THE DESIGN INVERT ELEVATION.
 - TRENCH PROTECTION SHALL BE REQUIRED IN ACCORDANCE WITH OSHA REGULATIONS, AND AS OTHERWISE REQUIRED TO PROTECT UTILITIES, ROADWAYS, AND ADJACENT STRUCTURES.
 - SEWER PIPES SHALL BE LAID BEGINNING AT THE DOWNSTREAM END OF THE PIPE LINE.
 - ALL PVC SEWER PIPES SHALL BE IPEX RING-TITE SDR 35, OR SIMILAR APPROVED.
 - ALL SEWER PIPE AND GASKETS SHALL CONFORM TO ASTM 3034 AND ASTM F679.
 - BACKFILL MATERIAL SHALL BE PLACED IN LAYERS NOT TO EXCEED 12" IN HEIGHT WHEN INSTALLED UNDER LANDSCAPED AREAS ONLY, INSTALLATIONS UNDER PAVEMENT REQUIRE BACKFILL MATERIAL TO BE PLACED IN LAYERS NOT TO EXCEED 6" IN HEIGHT. THESE LAYERS SHALL BE COMPACTED TO 95% MAXIMUM DENSITY (AASHTO T99). SUITABLE BACKFILL SHALL BE FREE OF LOAM, CLAY, ORGANIC MATTER AND PARTICLES LARGER THAN 2 INCHES IN DIAMETER.
 - SEWER PIPE TRENCHES SHALL BE BEDDED WITH CRUSHED STONE OR SCREENED GRAVEL. THESE MATERIALS MUST CONFORM TO RIDOT STANDARD M.01.09 TYPE II MATERIAL.
 - WATER PIPE TRENCHES MUST BE BEDDED WITH SAND CONTAINING NO PARTICLES LARGER THAN 3/8". THIS MATERIAL MUST CONFORM TO AASHTO M6 REQUIREMENTS.
 - UTILITY INSTALLATIONS SHALL CONFORM TO ALL REQUIREMENTS OF THE NEWPORT D.P.U. AND NEWPORT WATER.
 - WHEN TRENCH EXCAVATION IS ADJACENT TO OR UNDER EXISTING STRUCTURES OR FACILITIES, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPERLY SHEETING AND BRACING THE EXCAVATION AND STABILIZING THE EXISTING GROUND TO RENDER IT SAFE AND SECURE FROM POSSIBLE SLIDES, CAVE-INS AND SETTLEMENT AND FOR PROPERLY SUPPORTING EXISTING STRUCTURES AND FACILITIES WITH BEAMS, STRUTS OR UNDERPINNING TO FULLY PROTECT THEM FROM DAMAGE.



TYPICAL PERVIOUS ACCESS AND PARKING SECTION
SCALE: NOT TO SCALE



PRECAST 4'-0" ROUND MANHOLE DETAIL (RIDOT 4.2.0)
SCALE: NOT TO SCALE

- NOTES:**
- SHALL BE IN ACCORDANCE WITH SECTION 703 OF THE R.I. STANDARD SPECIFICATIONS.
 - CIRCUMFERENTIAL STEEL REINFORCEMENT REQUIRED = 0.12 SQ. IN. / LIN. FT. MINIMUM.
 - STEEL REINFORCEMENT FOR BASE SECTION BOTTOM SHALL BE A MINIMUM OF 0.12 SQ. IN./LIN. FT. (BOTH WAYS).
 - ONE FOUR MONOLITHIC BASE SECTION.
 - ANY NECESSARY ADJUSTMENTS DURING CONSTRUCTION WILL BE DONE BY SAW-CUTTING AND/OR CORING ONLY. NO JACKHAMMERS, HAMMERS AND CHISELS OR PNEUMATIC TOOLS WILL BE ALLOWED.
 - STEPS SHALL CONFORM TO STD. 5.3.0 AND SHALL BE INSTALLED AT THE CASTING PLANT.
 - ALTERNATE TOP SLAB IS STEEL REINFORCED TO MEET OR EXCEED H-26 LOADING (SEE STD. 4.7.2).
 - ALTERNATE TOP SLAB IS ONLY FOR USE WHEN REDUCING SECTION DOES NOT FIT BECAUSE OF STRUCTURE DEPTH.
 - REFER TO STD. 5.3.0 FOR MAXIMUM PIPE SIZES.

DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
REVIEWED SITE PLAN APPLICATION NO. 17-0163
DATED AUG 24 2017
SEE LETTER OF SAME DATE.

Cynthia D. Senack

No.	Revision	Date	App.

Designed By: JUR Drawn by: DJW Checked by: AS SHOWN Date: 13APR17

Project Title:
**A.P. 41, LOT 14
90 HARRISON AVENUE
FORMER UNDERWOOD SCHOOL
NEWPORT, RHODE ISLAND**

Client/Owner:
SCHOOLYARD PROPERTIES LLC
19 BRENTON ROAD
NEWPORT, RI 02840

Issued for:
RIDEM

Drawing Title:
DETAIL SHEET

Drawing Number:
D-1

Sheet 7 of 7

Project Number:
16068.1

Survey Index:
14 - 41 - 14

CERALYN E. SMALL
No. 8550
REGISTERED PROFESSIONAL ENGINEER
CIVIL

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