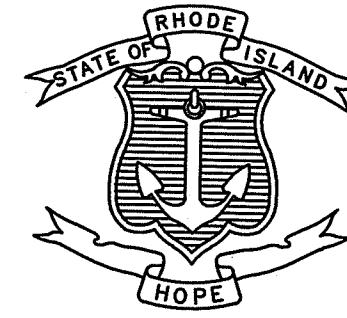


INDEX

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	STANDARD PLAN SYMBOLS & STANDARD LEGEND
3	STANDARD NOTES - 1
4	STANDARD NOTES - 2
5	JOB SPECIFIC PLAN SYMBOLS, LEGEND & NOTES
6	BRIDGE JOB SPECIFIC NOTES NO. 1
7	TYPICAL SECTION NO. 1
8	GENERAL PLAN NO. 1
9	MISCELLANEOUS DETAILS NO. 1
10	DRAINAGE PLAN NO. 1
11	PROFILE
12	BRIDGE GENERAL PLAN
13	DEMOLITION DETAILS

STATE OF RHODE ISLAND



DEPARTMENT OF TRANSPORTATION

PLAN, PROFILE AND SECTIONS OF PROPOSED
STATE HIGHWAY

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT

190'± TO 410'± NORTH OF THE INTERSECTION OF ROUTE 114 AND ROUTE 120

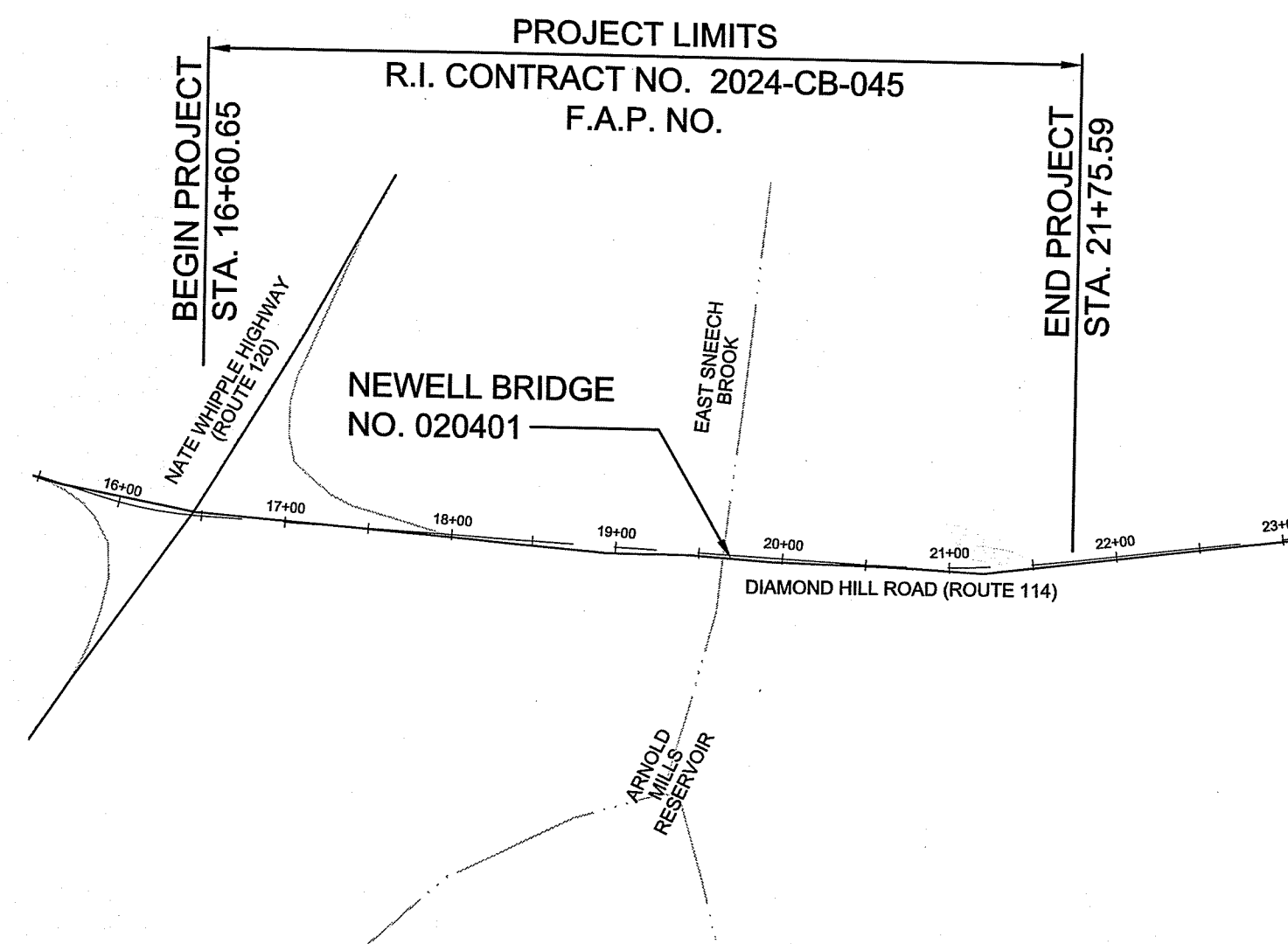
TOWN OF CUMBERLAND
COUNTY OF PROVIDENCE

R.I. CONTRACT NO. 2024-CB-045 F.A. PROJECT NO.

PAVEMENT STRUCTURE

3" MODIFIED CLASS 9.5 HMA WITH PAY ADJUSTMENTS SURFACE COURSE
6" CLASS 19.0 HMA BASE COURSE
12" GRAVEL BORROW SUBBASE COURSE

0.10 MILES

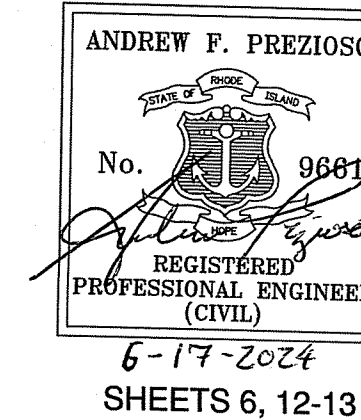
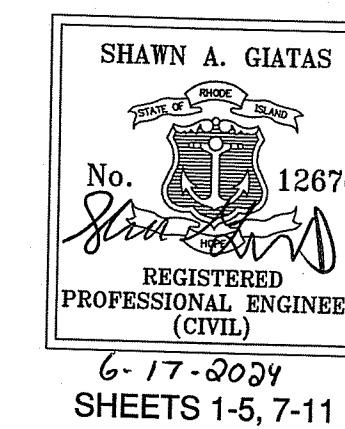


LAYOUT PLAN
SCALE: 1"=100'

SCALES OF DRAWINGS

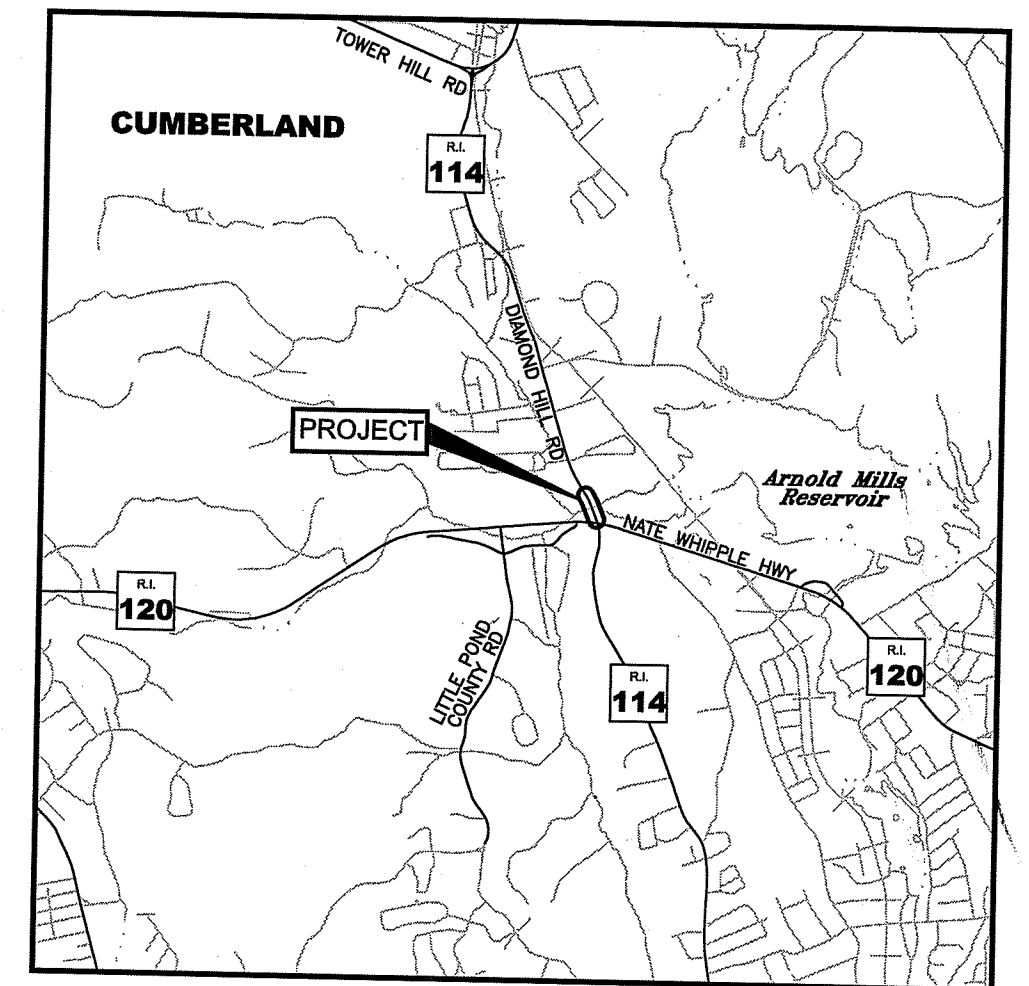
Plans	1 inch = 20 feet
Profiles	1 inch = 20 feet Horizontal
Profiles	1 inch = 4 feet Vertical
Cross Sections	1 inch = 4 feet Horizontal
Cross Sections	1 inch = 4 feet Vertical

BASE OF LEVELS
NGVD 29
NAD 83 (2011)



Contract Number 2024-CB-045
Number of Sheet 1
Total Sheets 13

RI CONTRACT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
2024-CB-045	2023	1	13

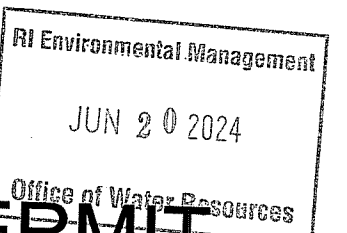


LOCATION MAP
SCALE

DESIGN DESIGNATION

	ROUTE 114	ROUTE 120
AADT (2023)	10,100 V.P.D.	6,100 V.P.D.
AADT (2048)	11,500 V.P.D.	7,000 V.P.D.
D	55%	50%
K	7.3%	9.0%
T	3.2%	3.2%
DHV (2023)	740 V.P.H.	550 V.P.H.
DHV (2048)	840 V.P.H.	630 V.P.H.
DESIGN SPEED	35 M.P.H.	45 M.P.H.

RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
APPROVED WITH CONDITIONS AS SPECIFIED IN THE LETTER OF APPROVAL
DATED: JUN 18 2024 FILE #: 24-0152
NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
APPROVED PLANS MUST BE AT CONSTRUCTION SITE
Dorey L. Freeman



RIDEM PERMIT
SUBMISSION
JUNE 2024

R.I. DEPARTMENT OF TRANSPORTATION

APPROVED _____ DATE _____

DIRECTOR, DIVISION OF PROJECT MANAGEMENT

APPROVED _____ DATE _____

CHIEF ENGINEER OF INFRASTRUCTURE

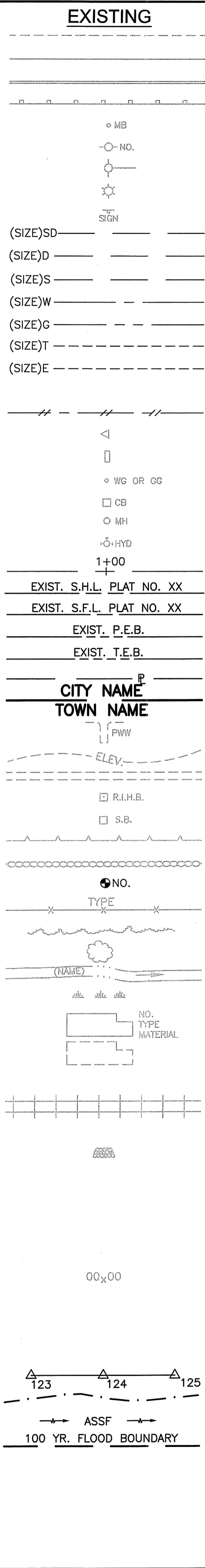
APPROVED _____ DATE _____

DIRECTOR _____ DATE _____

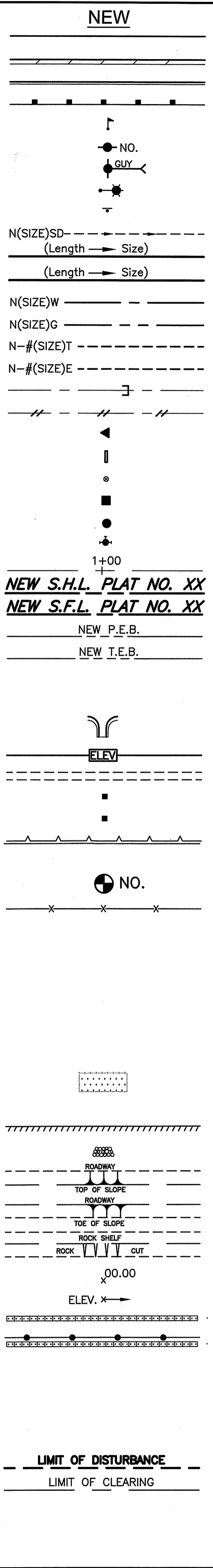
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED _____ DATE _____

DIVISION ADMINISTRATOR _____ DATE _____



EDGE OF PAVEMENT
 BERM
 CURB
 GUARDRAIL
 MAILBOX
 UTILITY POLE
 POLE GUY
 LUMINARE
 SIGN
 SUBDRAIN
 STORMDRAIN
 SANITARY SEWER
 WATER MAIN
 GAS MAIN
 TELEPHONE DUCT
 ELECTRIC DUCT
 PLUG AND CAP PIPE
 ABANDONED UTILITY
 FLARED END SECTION
 HEADWALL
 WATER OR GAS GATE
 CATCH BASIN
 MANHOLE
 HYDRANT
 BASELINE OR CENTERLINE
 STATE HIGHWAY LINE
 STATE FREEWAY LINE
 PERMANENT EASEMENT LINE
 TEMPORARY EASEMENT LINE
 PROPERTY LINE
 CITY OR TOWN LINE
 PAVED WATERWAY
 CONTOUR LINE
 OPEN DITCH
 R.I. HIGHWAY BOUND
 STONE BOUND
 RETAINING WALL
 FIELD STONE WALL
 BORINGS
 FENCE
 WOOD OR BRUSH LINE
 TREES
 RIVER OR STREAM
 WETLAND AREA
 BUILDING
 FOUNDATION
 BUILDING TO BE REMOVED
 RAILROAD TRACKS
 CUT AND MATCH
 RIP-RAP
 CUT SLOPE
 FILL SLOPE
 ROCK CUT
 SPOT GRADE
 AREA GRADED TO DRAIN
 BALED HAY RI STD 9.1.0
 BALED HAY & SILT FENCE RI STD. 9.3.0
 EDGE OF WETLAND
 WETLAND PERIMETER
 AREA SUBJECT TO STORM FLOW
 100-YEAR FLOOD PLAIN
 LIMIT OF DISTURBANCE
 LIMIT OF CLEARING

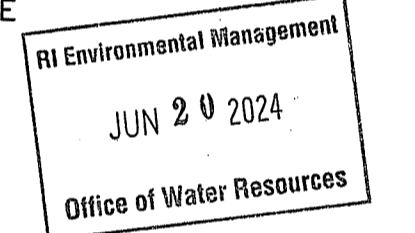


UNDERDRAIN
 CONCRETE CONNECTING COLLAR
 CONCRETE HEADWALLS FOR PIPE CULVERTS
 STANDARD HEADWALLS FOR MULTIPLE 3'-6" TO 7'-0" PIPE CULVERTS
 PRECAST CONCRETE FLARED END SECTION
 BRICK/SOLID BLOCK 4'-0" ROUND MANHOLE
 BRICK/SOLID BLOCK 5'-0" OR 6'-0" ROUND MANHOLE
 BRICK/SOLID BLOCK TYPE "D" SQUARE CATCH BASIN
 BRICK/SOLID BLOCK TYPE "F" SQUARE CATCH BASIN
 SOLID BLOCK FLUSH SQUARE CATCH BASIN
 BRICK/SOLID BLOCK TYPE "D" ROUND CATCH BASIN
 BRICK/SOLID BLOCK ROUND CATCH BASIN WITH GUTTER INLET
 BRICK/SOLID BLOCK TYPE "F" ROUND CATCH BASIN
 BRICK/SOLID BLOCK TYPE "R" CATCH BASIN
 SOLID BLOCK FLUSH ROUND CATCH BASIN
 BRICK/SOLID BLOCK 5'-0" OR 6'-0" ROUND CATCH BASIN
 SOLID BLOCK SHALLOW TYPE "F" SQUARE CATCH BASIN
 SOLID BLOCK SHALLOW 5'-0" OR 6'-0" SQUARE CATCH BASIN
 BRICK/SOLID BLOCK DROP INLET
 BRICK/SOLID BLOCK ROUND MANHOLE OR CATCH BASIN GREATER THAN 12'-0"
 PRECAST 4'-0" ROUND MANHOLE
 PRECAST 5'-0" ROUND MANHOLE
 PRECAST 6'-0" ROUND MANHOLE
 PRECAST 4'-0" OR 6'-0" SQUARE MANHOLE OR CATCH BASIN
 PRECAST 4'-0", 5'-0", OR 6'-0" ROUND CATCH BASIN
 PRECAST CONCRETE DROP INLET
 PRECAST CONCRETE DROP INLET LATERAL OUTLET
 PRECAST CONCRETE DROP INLET LONGITUDINAL OUTLET
 CATCH BASIN AND MANHOLE STEP
 CONCRETE COLLARS
 LIGHT-DUTY SQUARE FRAME AND ROUND COVER
 HEAVY DUTY SQUARE FRAME AND ROUND COVER
 LIGHT-DUTY ROUND FRAME AND COVER
 HEAVY-DUTY ROUND FRAME AND COVER
 SQUARE FRAME AND GRATE
 SQUARE FRAME AND GRATE
 SQUARE FRAME AND GRATE (BICYCLE SAFE)
 HIGH CAPACITY FRAME AND GRATE
 HIGH CAPACITY FRAME AND GRATE (BICYCLE SAFE)
 ROUND FRAME AND GRATE
 PRECAST CONCRETE CURB (STRAIGHT)
 PRECAST CONCRETE CURB (CIRCULAR)
 3'-0' PRECAST CONCRETE TRANSITION CURB
 6'-0" PRECAST CONCRETE TRANSITION CURB
 PRECAST 2'-0" RADIUS CORNER
 PRECAST CONCRETE INLET STONE (FOR SQUARE CATCH BASIN)
 PRECAST CONCRETE INLET STONE (FOR ROUND CATCH BASIN)
 PRECAST CONCRETE APRON STONE (FOR SQUARE CATCH BASIN)
 PRECAST CONCRETE APRON STONE (FOR ROUND CATCH BASIN)
 PRECAST CONCRETE SLOPED FACE CURB (STRAIGHT)
 PRECAST CONCRETE SLOPED FACE CURB (CIRCULAR)
 PRECAST CONCRETE SLOPED FACE TRANSITION CURB
 PRECAST CONCRETE TRANSITION CURB (VERTICAL FACE TO SPLOPED FACE)
 GRANITE CURB (STRAIGHT)
 GRANITE CURB (CIRCULAR)
 6'-0" GRANITE TRANSITION CURB
 GRANITE WHEELCHAIR RAMP TRANSITION CURB
 GRANITE 2'-0" RADIUS CORNER
 GRANITE INLET STONE (FOR SQUARE CATCH BASIN)
 GRANITE INLET STONE (FOR ROUND CATCH BASIN)
 GRANITE APRON STONE (FOR SQUARE CATCH BASIN)
 GRANITE APRON STONE (FOR ROUND CATCH BASIN)
 GRANITE SLOPED FACE CURB
 GRANITE SLOPED FACE TRANSITION CURB
 GRANITE TRANSITION CURB (VERTICAL FACE TO SLOPE FACE)
 BITUMINOUS CONCRETE LIP CURB

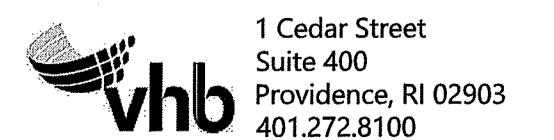
BITUMINOUS BERM
 CURB SETTING DETAIL
 BITUMINOUS CONCRETE DITCH
 RIP-RAP DITCH
 PAVED WATERWAY
 BALED HAY DITCH EROSION CHECK AND SILT FENCE COMBINED
 BALED HAY DITCH AND SWALE EROSION CHECK
 LOG AND HAY CHECK DAM
 DEWATERING BASIN
 BALED HAY CATCH BASIN INLET PROTECTION
 CONSTRUCTION ACCESS
 WET STONE MASONRY RETAINING WALL
 RUBBLE MASONRY WALL
 CONCRETE RETAINING WALL
 STONE MASONRY STEPS
 CONCRETE HIGHWAY BOUND
 POST AND MOUNTINGS FOR RURAL MAILBOX
 POST AND MULTIPLE MOUNTINGS FOR RURAL MAILBOXES
 PRECAST TYPE "A" HANDHOLE
 HEAVY DUTY TYPE "H" HANDHOLE
 ALUMINUM LIGHTING STANDARDS
 BI-DIRECTIONAL CONTROL DEVICE
 STREET SIGN MOUNTING DETAIL
 POLYETHYLENE DRUM WITH MARKINGS
 PVC PLASTIC PIPE TYPE III BARRICADE
 CHAIN LINK FENCE 3'-0" TO 4'-0"
 CHAIN LINK FENCE 5'-0" TO 6'-0"
 CHAIN LINK FENCE 5'-0" TO 6'-0" INTERMEDIATE POST
 WOVEN WIRE RIGHT-OF-WAY FENCE (STEEL POST)
 ROADSIDE GUARDRAIL (GENERAL NOTES, INSTALLATION, POST & OFFSET BLOCK DETAILS)
 TYPICAL GUARDRAIL INSTALLATION AT STRUCTURES
 STEEL BEAM GUARDRAIL ENCASED POST FOR SHALLOW INSTALLATION
 STEEL BEAM GUARDRAIL DEEP POST INSTALLATION
 STEEL BEAM GUARDRAIL INSTALLED IN CONCRETE OR HMA SURFACE
 STEEL BEAM GUARDRAIL, TL-3
 STEEL BEAM GUARDRAIL, TL-2
 STEEL BEAM GUARDRAIL DOUBLE FACE ASSEMBLY
 STEEL BEAM GUARDRAIL REFLECTORIZED TRIANGULAR DELINEATOR
 STEEL BEAM GUARDRAIL APPROACH END TREATMENT
 STEEL BEAM GUARDRAIL TERMINAL END SECTION
 STEEL BEAM GUARDRAIL ANCHORAGE TRAILING END SECTION
 STEEL BEAM GUARDRAIL THRIE BEAM TRANSITION PANEL
 STEEL BEAM GUARDRAIL CONNECTION TO NEW END POST GUARDRAIL CONNECTION TO EXISTING END POST APPROACH END SECTION
 GUARDRAIL CONNECTION TO EXISTING END POST TRAILING END SECTION
 STEEL BEAM GUARDRAIL TRANSITION TO RIGID BARRIER
 MASH GUARDRAIL TRANSITION TO EXISTING GUARDRAIL
 STEEL BEAM GUARDRAIL LONG SPAN, TL-3
 STEEL THRIE BEAM GUARDRAIL SINGLE FACE
 STEEL THRIE BEAM GUARDRAIL DOUBLE FACE
 STEEL THRIE BEAM GUARDRAIL LONG SPAN
 F SHAPE CONCRETE BARRIER DOUBLE FACE
 F SHAPE CONCRETE BARRIER SINGLE FACE
 F SHAPE CONCRETE BARRIER WITH CONCRETE SEPARATOR
 PRECAST MEDIAN BARRIER TRANSITION UNIT
 PRECAST MEDIAN BARRIER FOR LIGHT STANDARD
 BARRIER MOUNTED DELINEATOR
 SINGLE-FACED PRECAST MEDIAN BARRIER
 PRECAST MEDIAN BARRIER TRANSITION UNIT
 BARRIER MOUNTED DELINEATOR
 CEMENT CONCRETE SIDEWALK
 BITUMINOUS CONCRETE SIDEWALK
 WHEELCHAIR RAMP
 WHEELCHAIR RAMP FOR LIMITED RIGHT-OF-WAY AREAS
 DRIVEWAY DEVELOPMENT FOR 3'-0" TRANSITION CURB
 DRIVEWAY DEVELOPMENT FOR 6'-0" TRANSITION CURB

CEMENT CONCRETE DRIVEWAYS
 DETECTABLE WARNING SYSTEM
 TREE PROTECTION DEVICE
 DRIP LINE TREE PROTECTION DEVICE FOR EXISTING TREES
 SHRUB PROTECTION DEVICE
 TREE WELL
 TREE WALL
 ADJUST CATCH BASIN TO GRADE
 ADJUST CATCH BASIN TO MANHOLE
 ADJUST CURB STOP TO GRADE
 ADJUST DRAINAGE MANHOLE TO GRADE
 ADJUST ELECTRIC MANHOLE TO GRADE
 ADJUST FRAME AND COVER TO GRADE
 ADJUST FRAME AND GRATE TO GRADE
 ADJUST GAS GATE BOX TO GRADE
 ADJUST HANDHOLE TO GRADE
 ADJUST SANITARY SEWER MANHOLE TO GRADE
 ADJUST TELEPHONE MANHOLE TO GRADE
 ADJUST WATER GATE BOX TO GRADE
 BITUMINOUS CONCRETE DRIVEWAY
 3" CLASS 9.5 HMA
 8" GRAVEL BORROW SUBBASE COURSE
 BUILD NEW STRUCTURE OVER EXISTING PIPE
 CLEAN CATCH BASIN
 CUT AND CAP PIPE WITH RESTRAINT (ALL SIZES)
 CLEAN AND FLUSH PIPE
 CLEARING AND GRUBBING
 CLEAN MANHOLE
 CP (DEPTH) COLD PLANE
 CUT AND PLUG PIPE (ALL TYPES, ALL SIZES)
 REMOVE AND DISPOSE BITUMINOUS CURB
 REMOVE AND DISPOSE CONCRETE CURB
 REMOVE AND DISPOSE CATCH BASIN
 REMOVE AND DISPOSE DROP INLET
 REMOVE AND DISPOSE FENCE
 REMOVE AND DISPOSE FRAME AND COVER
 REMOVE AND DISPOSE FLARED END SECTION
 REMOVE AND DISPOSE FRAME AND GRATE
 REMOVE AND DISPOSE FIRE HYDRANT
 REMOVE AND DISPOSE FLEXIBLE PAVEMENT
 REMOVE AND DISPOSE GUARDRAIL
 REMOVE AND DISPOSE HEADWALL
 REMOVE AND DISPOSE HIGHWAY BOUND
 REMOVE AND DISPOSE HANDHOLE
 REMOVE AND DISPOSE LIGHT AND FOUNDATION
 REMOVE AND DISPOSE MEDIAN BARRIER
 REMOVE AND DISPOSE MANHOLE
 REMOVE AND DISPOSE MEDIAN MARKER
 REMOVE AND DISPOSE OBSERVATION WELL
 REMOVE AND DISPOSE PIPE
 REMOVE AND DISPOSE PAVEMENT AND RIGID BASE
 REMOVE AND DISPOSE RIGID BASE
 REMOVE AND DISPOSE SIGN
 REMOVE AND DISPOSE TRAFFIC SIGNAL SYSTEM
 REMOVE AND DISPOSE SIDEWALK
 REMOVE AND DISPOSE TELEPHONE DUCT BANKS
 REMOVE AND DISPOSE UTILITY POLE
 REMOVE AND DISPOSE PAVED WATERWAY
 FILTER FABRIC RIPRAP FLARED END UNDERLAYMENT
 FLARED GUARDRAIL END TREATMENT
 IMPACT ATTENUATOR
 IMPERVIOUS DITCH LINER
 LIMIT OF DISTURBANCE
 LIMIT OF REGRADING
 4" LOAM AND SEED
 NEW FIRE HYDRANT WITH GATE VALVE

NOT IN THIS CONSTRUCTION CONTRACT
 FURNISH AND INSTALL NEW WATER GATE VALVE BOX
 FURNISH AND INSTALL NEW WATER GATE VALVE AND BOX
 FURNISH AND INSTALL NEW WATER CURB STOP BOX
 FURNISH AND INSTALL NEW WATER CURB STOP AND BOX
 PERMANENT CHECK DAM
 4" PLANTABLE SOIL AND SEED
 RECONSTRUCT TYPE "D" CATCH BASIN, TO CATCH BASIN WITH GUTTER INLET
 R.I.D.O.T. COMMUNICATIONS MANHOLE
 REMOVE, HANDLE, HAUL, TRIM, RESET CURB EDGING, STRAIGHT, CIRCULAR (ALL TYPES)
 RELOCATE LAMP POST
 RELOCATE MAILBOX (BY OTHERS)
 REMOVE PAVEMENT MARKINGS
 RIP-RAP PAD (SEE DETAIL)
 REMOVE AND RELOCATE SIGN
 RELOCATE UTILITY POLE (BY OTHERS)
 STONE BAFFLE
 STEEL BEAM BRIDGE CONNECTION APPROACH END (W/O NESTED RAIL)
 STEEL BEAM BRIDGE CONNECTION TRAILING END (W/NESTED RAIL)
 STRUCTURAL DISPOSITION - SEE CS PAGES OF SPECIFICATION
 REMOVE AND STOCKPILE FENCE
 SPECIAL GRADED AGGREGATE
 REMOVE AND STOCKPILE GRANITE CURB
 REMOVE AND STOCKPILE GUARDRAIL
 REMOVE AND STOCKPILE HYDRANT
 REMOVE AND STOCKPILE SIGN
 REMOVE AND STOCKPILE TRAFFIC SIGNAL SYSTEM
 CONCRETE THRUST BLOCK
 TIE EXISTING PIPE INTO NEW STRUCTURE
 TIE NEW PIPE INTO EXISTING STRUCTURE
 THRIE BEAM TRANSITION
 THRIE BEAM BRIDGE CONNECTION
 TREE TRIMMING
 4" WOOD CHIP MULCH
 4" EPOXY RESIN PAVEMENT MARKINGS - DOUBLE YELLOW
 6" EPOXY RESIN PAVEMENT MARKINGS - WHITE
 12" EPOXY RESIN PAVEMENT MARKINGS - WHITE
 6" PREFORMED PATTERNED MARKING (HIGH PERFORMANCE TAPE)
 4" EPOXY RESIN PAVEMENT MARKINGS - YELLOW
 6" EPOXY RESIN PAVEMENT MARKINGS - YELLOW
 PROFILE GRADE LINE



RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
 OFFICE OF WATER RESOURCES
 FRESHWATER WETLANDS PROGRAM
 APPROVED WITH CONDITIONS AS SPECIFIED IN THE LETTER OF APPROVAL
 DATED: JUN 10 2024 FILE #: 24-0152
 NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
 APPROVED PLANS MUST BE AT CONSTRUCTION SITE
 Nancy L. Freeman



RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
 CHECKED BY:
 DATE:
 SHEET: 2
 OF: 1215

SCALE: NOT TO SCALE

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
1	6/07	TRB			
2	12/22	RS			

BRIDGE GROUP 17C -
 NEWELL BRIDGE REPLACEMENT
 CUMBERLAND RHODE ISLAND
 STANDARD PLAN SYMBOLS &
 STANDARD LEGEND

GENERAL NOTES:

- ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, DRAINAGE STRUCTURES, DRAINAGE PIPES, INFILTRATION AREAS, ROADSIDE, CONDUIT, SIDEWALK, FENCES, ETC., CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
- THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.05 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION. EQUIPMENT AND MATERIAL SHALL NOT BE STORED IN AREAS DESIGNATED FOR STORMWATER INFILTRATION OR OUTSIDE THE L.O.D. WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT DISTURBED OR OBLITERATED BEFORE SURVEY GROUND CONTROL POINTS ARE LOCATED, VERIFIED, AND DEEMED ADEQUATE FOR CONSTRUCTION LAYOUT. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING THE CONTRACTOR TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT BEGIN CONSTRUCTION ACTIVITIES UNTIL ADEQUATE SURVEY GROUND CONTROL POINTS HAVE BEEN ESTABLISHED, TIED DOWN, AND VERIFIED IN WRITING BY THE CONTRACTOR'S PROFESSIONAL LAND SURVEYOR.
- ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
- THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE DETERMINED BY THE CONTRACTOR TO MEET THE REQUIREMENTS OF SECTION 907.
- ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANE PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 7 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE INCIDENTAL TO THE APPLICABLE PAVEMENT ITEMS.
- THE LIMITS OF CLEARING AND SURFACE DISTURBANCE SHALL BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT ITS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED.
- THE CONTRACTOR WILL NOT BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
- CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER. CLEANING WITH COMPRESSED AIR SHALL ONLY BE ALLOWED WITH THE APPROVAL OF THE ENGINEER.
- PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS AND SHOP DRAWINGS OR AS MODIFIED BY THE ENGINEER.
- THE COORDINATE SYSTEM, IF SHOWN, IS THE RHODE ISLAND STATE PLANE COORDINATE SYSTEM.
- PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY-TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO MAINTAIN THE SAFE TRAVEL OF THE PUBLIC AT NO ADDITIONAL COST TO THE STATE.
- NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO NEW AND EXISTING DRAINAGE STRUCTURES HAS BEEN PROPERLY ESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; ANY CORRECTIVE ACTION SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
- ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
- THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM), AND/OR THE ARMY CORPS OF ENGINEERS (ACOE), AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC). COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).

GENERAL NOTES (CONTINUED):

- FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL ACTION WORK AND/OR SOIL MANAGEMENT PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
- THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

- THE CONTRACTOR IS REQUIRED TO ADHERE WITH THE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIPPEE GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE SPECIFIC SWPPP FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE SWPPP AS SITE CONDITIONS WARRANT. A COPY OF THE SWPPP MUST BE ON-SITE AT ALL TIMES. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS.
- NO UNDISTURBED AREAS SHALL BE GRUBBED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE RESTABILIZED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
- STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODIBLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES TO STABILIZE. STOCKPILES OF CONTAMINATED MATERIALS MUST BE PLACED ON TOP OF A POLY-ETHYLENE SHEET AND COVERED AT ALL TIMES UNLESS IT IS AN ACTIVE WORKING PILE.
- IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION DEWATERING BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. ENVIRONMENTAL DIVISION.
- SURFACE EROSION CONTROL MATTING SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
- SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - SEEDING TYPE I.
 - ADHESIVE MULCH STABILIZER
- UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
- PRIOR TO CONSTRUCTION OPERATIONS, THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING THE PIPES, AND THEN VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION.
- ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER SHUTDOWN.
- DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE EFFICACY OF THE DRAINAGE SYSTEM. ONCE CONSTRUCTION IS COMPLETED THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING ALL PIPES OF ANY CONSTRUCTION RELATED DEBRIS AT NO ADDITIONAL COST.
- CATCH BASIN RIM GRADES FOR STRUCTURES NOT IN A TRAVEL LANE NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
- PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL STRICTLY ADHERE TO THE PLANS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL INSTALL ALL SEDIMENT AND EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. SEDIMENT AND EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING.
- R.I. STD. 9.8.0 BALED STRAW INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
- WHERE BALED STRAW INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):

- DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN REMOVED AND INFILTRATION IS REESTABLISHED, FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED TO COMPLY WITH THE PLANS, SPECIFICATIONS, AND PERMITS. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION AND COSTS REQUIRED TO RESOLVE SUCH ISSUES IS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
- PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
- ALL COMPOST FILTER SOCK, STRAW BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
- THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
- THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEMS. ADDITIONAL SEDIMENT AND EROSION CONTROLS, SHALL BE INSTALLED IN ACCORDANCE WITH THE SWPPP REPORT. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.
- ANY OBSERVATIONS OF ILLICIT CONNECTIONS OR DISCHARGES TO RIDOT'S DRAINAGE NETWORK OR OUTFALLS SHALL BE REPORTED TO THE RIDOT STORMWATER UNIT IMMEDIATELY.

UTILITY NOTES:

- EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
- THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39-1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
- ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
- EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW WATER MAINS.
- UTILITY SERVICE CONNECTIONS SHALL BE MAINTAINED TO ALL EXISTING FACILITIES TO REMAIN.
- FIRE HYDRANTS SHALL NOT BE REMOVED FROM SERVICE WITHOUT WRITTEN AUTHORIZATION FROM THE FIRE DEPARTMENT OR THE WATER AUTHORITY.
- ALL NEW WATER LINES SHALL BE DISINFECTED TO THE SATISFACTION OF THE WATER AUTHORITY IN ACCORDANCE WITH THE SPECIFICATIONS.
- ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.
- THE CONTRACTOR SHALL PROVIDE 72-HOUR ADVANCE NOTICE TO THE RIDOT TMC (401-222-2378) FOR WORK AROUND RIDOT OWNED INFRASTRUCTURE (DRAINAGE, LIGHTING, ITS EQUIPMENT, TOLL GANTRIES, COUNTING STATIONS, ETC.). ANY DAMAGE TO THIS INFRASTRUCTURE MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT RIDOT IN ADVANCE, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.

RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
APPROVED WITH CONDITIONS AS
SPECIFIED IN THE LETTER OF APPROVAL
DATED: July 18, 2023 FILE # 24-0152
NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
APPROVED PLAN BRIDGE GROUP 17C - NEWELL BRIDGE REPLACEMENT SITE



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE:
SHEET: 3
OF: 3213

SCALE: NOT TO SCALE

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
1	4/07	TRB	4	12/22	JRP
2	3/10	RBH			
3	4/14	MLP			

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT

CUMBERLAND RHODE ISLAND

STANDARD NOTES - 1

LANDSCAPE NOTES:

- ALL PLANT MATERIAL MUST BE TAGGED AT THE NURSERY (A RECOGNIZED GROWER OF PLANT MATERIAL) IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE NURSERY GROWN; NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
- ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
- COORDINATE WITH THE R.I.D.O.T. CONSTRUCTION MANAGER PRIOR TO ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
- ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
- PROVIDE A MINIMUM 6'-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.
- THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THERE ARE NO CONTAMINANTS THAT EXCEED THE R.I.D.E.M. PERMISSIBLE LEVELS IN THE SOILS USED AS LOAM OR PLANTABLE SOIL.

STRUCTURAL NOTES FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS:

GENERAL

- ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION AND REVISIONS OF THE AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS, INCLUDING THE LATEST INTERIM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

CONSTRUCTION DRAWINGS AND DETAILS

- THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SHOP DRAWINGS IN REFERENCE TO ANCHOR BOLTS:
 - "PRETENSIONING OF ALL ANCHOR NUTS IS REQUIRED, AND SHALL BE ACCOMPLISHED BY TIGHTENING TO 1/6TH TURN BEYOND THE SNUG-TIGHT POSITION."
 - "THE MAXIMUM CLEARANCE BETWEEN THE BOTTOM OF THE LEVELING NUTS AND THE TOP OF THE CONCRETE IS CRITICAL AND SHALL NOT EXCEED THE AMOUNT SPECIFIED ON THIS DRAWING."
- THE USE OF GROUT UNDER BASE PLATES SHALL GENERALLY NOT BE PERMITTED. IF SPECIFIC CONDITIONS WARRANT ITS USE, THE GROUT SHALL NOT BE CONSIDERED LOAD CARRYING; LOADS SHALL BE DIRECTLY SUPPORTED BY THE ANCHOR BOLTS. ADEQUATE DRAINAGE SHALL BE PROVIDED.
- THE DAMPENING EFFECTS OF VIBRATION MITIGATION DEVICES SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SIGNS AND TRAFFIC SIGNALS. IF THE CONTRACTOR CHOOSES TO USE THESE DEVICES FOR WARRANTY PURPOSES, THE TYPE OF DEVICES PROPOSED SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

TRAFFIC SIGNAL NOTES:

- ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE DELIVERED TO THE R.I.D.O.T. MAINTENANCE HEADQUARTERS, 360 LINCOLN AVENUE, WARWICK, RHODE ISLAND, 02888. THE COST FOR DELIVERY IS CONSIDERED INCIDENTAL TO THE WORK.
- BACK PLATES SHALL BE INSTALLED ON ALL TRAFFIC SIGNAL HEADS.
- THE CONTRACTOR SHALL SUPPLY AND INSTALL ON THE UPPER LEFT HAND CORNER OF THE BACK OF THE CONTROLLER CABINET DOOR A LAMINATED INTERSECTION GRAPHIC AND TABLE DEPICTING THE TRAFFIC DETECTOR RELAY CHANNEL ASSIGNMENTS. THE DIAGRAM SHALL BE A GRAPHIC OF THE INDIVIDUAL INTERSECTION ORIENTED SIMILAR TO THE PLANS SHOWING THE LOCATIONS OF EACH OF THE LOOP DETECTORS. THE DIAGRAM SHALL, AT A MINIMUM, INCLUDE DETECTOR NUMBERS, STREET NAME LABELS, NORTH ARROW, AND CONTROLLER CABINET LOCATION. THE ASSIGNMENT INFORMATION SHALL BE INCLUDED IN A TABLE WHICH SHALL INCLUDE, AT A MINIMUM, THE APPROACH NAME, DETECTOR NUMBER, TERMINAL NUMBER, DETECTOR RACK SLOT NUMBER, RELAY NUMBER, RELAY CHANNEL NUMBER, AND PHASE ASSOCIATED WITH EACH DETECTOR.
- TRAFFIC CONTROLLER CABINETS, UNLESS OTHERWISE NOTED, SHALL BE NEMA TS2 TYPE 1 CABINET SIZE 6 ("P" TYPE) WITH NOMINAL DIMENSIONS OF 52"Hx44"Wx24"D.
- ALL DELAY AND EXTENSION TIMES, AS CALLED FOR ON THE PLANS, FOR PROPOSED LOOP DETECTORS SHALL BE PROGRAMMED IN THE TRAFFIC SIGNAL CONTROLLER AND NOT THE DETECTOR RELAY.
- INSULATED GROUND WIRE SHALL BE PLACED IN ALL PVC CONDUITS AND SHALL BE BONDED TO GROUND RODS IN ACCORDANCE WITH SECTION T.03 OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- THE FINAL POSITION OF SIGNAL HEADS, PEDESTRIAN PUSHBUTTONS, DETECTORS, AND STOP LINE AND CROSSWALK PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER IN THE FIELD ACCORDING TO ACTUAL INTERSECTION CHARACTERISTICS.
- A 2' MINIMUM BUFFER SHALL BE PROVIDED BETWEEN THE CURB AND ALL LATERAL OBSTRUCTIONS (INCLUDING ALL SIGNAL POLES AND TRAFFIC/PEDESTRIAN SIGNAL HEADS) TO PROVIDE ADEQUATE CLEARANCE FOR TURNING VEHICLES.
- ALL FOUNDATIONS MUST HAVE CONES OR BARRELS BOLTED TO FOUNDATION BASES UNTIL ACTUAL POLE IS INSTALLED.
- WHEN PLACING TRAFFIC SIGNAL HANDHOLES OR CONDUIT IN EXISTING PORTLAND CEMENT CONCRETE SIDEWALKS, THE ENTIRE SIDEWALK SQUARE OF CONCRETE SHALL BE REPLACED IN ACCORDANCE WITH R.I. STD. 43.1.0. NO PATCHES WILL BE ALLOWED.
- ALL PEDESTRIAN PUSHBUTTONS SHALL BE COMPLIANT WITH "THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES" (ADAAG) AND SHALL INCLUDE A PRESSURE-ACTIVATED (NON-MOVING) BUTTON. SIGNS APPLICABLE TO PUSHBUTTON ACTUATION SHALL BE INSTALLED SUCH THAT THE CROSSING ASSIGNED TO EACH BUTTON IS CLEARLY INDICATED. IF SITE CONDITIONS DO NOT ALLOW PEDESTRIAN PUSHBUTTONS TO BE INSTALLED WHERE CALLED FOR ON THE PLANS, THE R.I.D.O.T. TRAFFIC ENGINEERING UNIT SHALL BE CONSULTED WITH THROUGH AN R.F.I. PRIOR TO INSTALLING THE PUSHBUTTONS. THE FINAL PLACEMENT OF ALL PEDESTRIAN PUSHBUTTONS SHALL BE IN ACCORDANCE WITH ADAAG AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL LOOP DETECTORS SHALL BE CENTERED WITHIN EACH LANE AS DELINEATED, UNLESS OTHERWISE DIMENSIONED ON PLANS.
- ALL LOOP DETECTORS SHALL BE CUT INTO THE FINAL PAVEMENT SURFACE COURSE.
- TRAFFIC SIGNAL CONTROLLERS AND CABINETS SHALL BE PROGRAMMED AND WIRED SO THAT ANY FIRE PRE-EMPTION SHALL OVERRIDE MANUAL (PUSH BUTTON) OPERATION.
- THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
- ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED, AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
- POLICE OFFICERS AND FLAGPERSONS SHALL BE UTILIZED AS OUTLINED IN SECTIONS 913 & 914 OF THE RI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- POLYETHYLENE DRUMS SHALL BE UTILIZED AS A CHANNELIZING DEVICE WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN BEYOND WORKING HOURS WHEN NO WORKERS ARE PRESENT. CONES SHALL BE UTILIZED WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN ONLY DURING WORKING HOURS AND IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
- ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR A MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER WORKZONE TRAFFIC CONTROL DEVICES THAT ARE DAMAGED OR REQUIRE RELOCATION SHALL BE REPLACED AND / OR RELOCATED UNDER THE PAY ITEM FOR "MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION."
- THE PRIVATE VEHICLES OF CONSTRUCTION WORKERS SHALL NOT BE PARKED ON THE TRAVEL LANES OR SHOULDERS. THEY MAY BE PARKED WITHIN THE STATE RIGHT-OF-WAY ONLY IN AREAS BEYOND THE OUTSIDE EDGE OF THE TRAVEL LANES AND/OR IN AREAS APPROVED BY THE ENGINEER.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.
- THE INTENDED VEHICLE PATHS THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES. APPROVED PAVEMENT MARKINGS SHALL BE INSTALLED BEFORE THE END OF THE WORK SHIFT ON ALL COLD-PLANNED AND NEW ROADWAY SURFACES THAT WILL BE OPENED TO TRAFFIC AT THE END OF THE SHIFT. FAILURE TO COMPLY WILL RESULT IN AN ASSESSMENT OF A CHARGE AS OUTLINED IN SECTION 937 OF THE RI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

RI Environmental Management
JUN 20 2024
Office of Water Resources

RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
APPROVED WITH CONDITIONS AS SPECIFIED IN THE LETTER OF APPROVAL
DATED: JUL 18 2024 FILE #: 24-0152
NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
APPROVED PLANS MUST BE AT CONSTRUCTION SITE
Nancy L. Freeman



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE:
SHEET: 4
OF: 13

SCALE: NOT TO SCALE








REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
1	4/07	TRB	4	12/22	JRP
2	11/07	TRB			
3	3/10	RBH			

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT
CUMBERLAND RHODE ISLAND


STANDARD NOTES - 2

RI CONTRACT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
2024-CB-045	2023	5	13

JOB SPECIFIC LEGEND:

-  COMPOST FILTER SOCK
-  FULL DEPTH PAVEMENT
3.0" MODIFIED CLASS 9.5 HMA WITH PAY ADJUSTMENTS
6.0" CLASS 19.0 HMA
12.0" GRAVEL BORROW SUBBASE
-  INLET PROTECTION
-  MICRO MILL
3.0" MICRO MILLING
OVERLAY
3.0" MODIFIED CLASS 12.5 HMA WITH PAY ADJUSTMENTS
-  PLACEMENT OF MILLINGS BELOW GUARDRAIL
-  RECONSTRUCT CORBEL CONE
-  STORM WATER TREATMENT DEVICE - NUMBER

JOB SPECIFIC LINE LEGEND:

-  COMPOST FILTER SOCK

GENERAL NOTES - PAVEMENT, SIDEWALK, AND CURBING

- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT DAMAGE TO BUILDINGS, WALLS, FENCES ABUTTING SIDEWALKS AND DRIVEWAYS DESIGNATED FOR REPLACEMENT. WHERE REQUIRED, NEW SIDEWALKS AND DRIVEWAYS SHALL MEET SAID BUILDINGS, WALLS, AND FENCES. PRIOR TO REMOVAL, A SAWCUT SHALL BE PROVIDED IN ALL SIDEWALK AND DRIVEWAY TO BE REMOVED, A DISTANCE TO BE DETERMINED BY THE ENGINEER (6 INCHES MINIMUM) OF SIDEWALK OR DRIVEWAY SHALL BE REMOVED WITH CAUTION UNDER THE ENGINEER'S SUPERVISION. THERE WILL BE NO ADDITIONAL PAYMENT FOR LABOR OR EQUIPMENT NECESSARY TO MEET THIS "REMOVE WITH CAUTION" REQUIREMENT.

DRAINAGE AND EROSION CONTROL NOTES

- COMPOST FILTER SOCK SHALL BE USED FOR PERIMETER EROSION CONTROLS AND SILT FENCE SHALL BE USED FOR TURBIDITY CONTROL AS NEEDED TO ADHERE TO THE SWPPP AND RIDEM REGULATIONS. CONTRACTOR SHALL PLACE COMPOST FILTER SOCK OR SILT CURTAIN AS NEEDED OR AS DIRECTED BY THE ENGINEER DEPENDENT ON THE CONSTRUCTION PHASING.
- ALL RIM ELEVATIONS SHOWN ON THE PLANS ARE SURFACE ELEVATION, RIMS SHALL BE CONSTRUCTED 0.1' BELOW THE RIM ELEVATION INDICATED ON THE PLANS. (THIS NOTE CLARIFIES DRAINAGE AND EROSION CONTROL NOTE 11 ON STANDARD NOTES-1).
- ALL REINFORCED CONCRETE DRAINAGE PIPES SHALL BE CLASS III UNLESS OTHERWISE NOTED ON THE PLANS.
- PIPE INVERTS AND SLOPES PROVIDED ARE CONCEPTUAL. FINAL DESIGNER TO PROVIDE PIPE CALCULATIONS TO VERIFY PRELIMINARY DESIGNS.

GENERAL NOTES - PAVEMENT MARKINGS:

- ALL PERMANENT PAVEMENT MARKINGS FOR THIS PROJECT SHALL BE EPOXY RESIN. PAVEMENT MARKINGS SHALL BE PLACED ON THE FINAL SURFACE COURSE NO SOONER THAN 2 WEEKS BUT NO LONGER THAN 4 WEEKS FROM COMPLETION OF PAVING OPERATIONS.
- THE LOCATION OF PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION, AS AMENDED.
- WHERE EXISTING PAVEMENT MARKINGS CONFLICT WITH PROPOSED PAVEMENT MARKINGS, EXISTING MARKINGS SHALL BE REMOVED BY METHOD APPROVED BY THE RIDOT. THE COST OF PAVEMENT MARKING REMOVAL SHALL BE CONSIDERED INCIDENTAL TO ITEM CODE T20.0101 PAVEMENT MARKINGS.
- LIMITS OF PROPOSED PAVEMENT MARKINGS SHALL MEET EXISTING STRIPING, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL VERIFY AND RECORD PAVEMENT MARKING LOCATIONS PRIOR TO ANY PAVEMENT REMOVAL. MARKINGS SHALL BE REPLACED IN ORIGINAL LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- FAST-DRYING WATERBORNE TRAFFIC MARKINGS SHALL BE USED ON MICRO MILLED SURFACES AND INTERMEDIATE PAVEMENT LAYERS WHICH WILL BE OPENED TO TRAFFIC AT THE COMPLETION OF EACH DAY'S PAVING OPERATION.
- TEMPORARY WATERBORNE PAVEMENT MARKINGS SHALL BE PLACED ON THE FINAL SURFACE COURSE LAYER WHICH WILL BE OPENED TO TRAFFIC AT THE COMPLETION OF EACH DAY'S PAVING OPERATION.

GENERAL NOTES - SIGNS:

- ALL NEW DIRECTIONAL, REGULATORY, WARNING, GUIDE SIGNS AND PARKING SIGNS SHALL HAVE SIGN SUPPORTS. UNLESS OTHERWISE INDICATED, SIGN MOUNTINGS SHALL BE R.I. STD. 24.2.0 OR 24.6.0 AS APPROPRIATE.
- PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE APPROVED OR MODIFIED BY THE RIDOT.
- ALL PROPOSED AND RELOCATED SIGNS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 7' OVER THE SIDEWALK. FOR SIGNS TO BE RELOCATED, IF 7' OF CLEARANCE CANNOT BE ACHIEVED USING THE EXISTING POST, A NEW POST SHALL BE USED. THE COST OF THE NEW POST SHALL BE CONSIDERED INCIDENTAL TO ITEM T15.0200 - REMOVE AND RELOCATE DIRECTIONAL REGULATORY OR WARNING SIGN.
- ALL SIGN RADII AND BORDERS SHALL BE AS SPECIFIED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS AMENDED.

GENERAL NOTES

- ALL ITEMS NOT REFERENCED FOR MODIFICATIONS WILL BE CONSIDERED "EXISTING TO REMAIN" UNLESS OTHERWISE NOTED.
- GUARDRAIL POSTS SHALL NOT BE PLACED IN OR THROUGH EXISTING OR PROPOSED DRAINAGE FEATURES INCLUDING BUT NOT LIMITED TO PAVED WATERWAYS, DRAINAGE PIPES AND DRAINAGE STRUCTURES. FINAL DESIGNER SHALL EVALUATE THE NEED FOR LONG SPANS BASED OFF EXISTING AND PROPOSED POST SPACING.
- EXISTING DELINEATORS WITHIN THE PROJECT LIMITS MAY NEED TO BE REMOVED AND RESET DUE TO THE INSTALLATION OF THE NEW GUARDRAIL RUN. THE CONTRACTOR SHALL REMOVE AND RESET DELINEATORS WHERE THEY CONFLICT WITH THE PROPOSED GUARDRAIL TO THE DIMENSIONS AND SPACING AS SHOWN ON SIGNING & STRIPING DETAILS PLAN AND IN CHAPTER 3F OF THE MUTCD. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE COST OF GUARDRAIL INSTALLATION.
- EXISTING DELINEATORS THAT ARE DETERMINED BY THE ENGINEER TO BE MISSING OR DAMAGED SHALL BE REPLACED TO THE DIMENSIONS AND SPACING AS SHOWN ON SIGNING & STRIPING DETAILS PLAN AND IN CHAPTER 3F OF THE MUTCD. EXISTING DELINEATORS THAT ARE DETERMINED BY THE ENGINEER TO NO LONGER BE NEEDED SHALL BE REMOVED AND DISPOSED. THIS WORK SHALL BE PAID FOR UNDER THE RESPECTIVE BID ITEMS "REMOVAL AND DISPOSAL OF MISCELLANEOUS OBJECTS" AND "DELINEATORS."

GENERAL NOTES - SURVEY

- FIELD SURVEY WORK WAS PERFORMED BY CROSSMAN ENGINEERING DURING MARCH 2020 AND DECEMBER 2021.
- FINAL DESIGN CONSULTANT SHALL EVALUATE THE NEED FOR ADDITIONAL SURVEY CONTROL AS NEEDED.


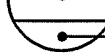

GENERAL NOTES - UTILITIES

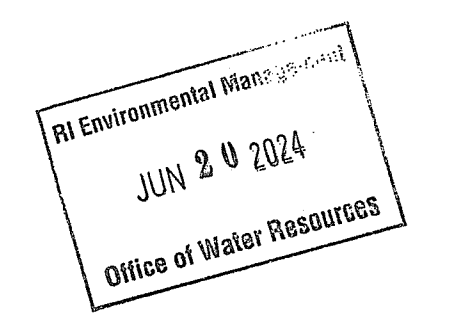
- CONTRACTOR SHALL FOLLOW THE GUIDELINES LISTED IN RI ENERGY'S "GUIDELINES FOR WORKING AROUND GAS UTILITIES", DOCUMENT ATTACHED.
- DEPTH OF GAS FACILITIES ARE UNKNOWN AND COULD BE SHALLOW, USE CAUTION WHEN WORKING IN THE VICINITY OF ANY GAS FACILITY, HAND DIGGING OR VACUUM EXCAVATION ONLY.
- RI ENERGY REQUIRES A MINIMUM OF ONE FOOT OF SEPARATION BETWEEN CROSSING UTILITIES AND EXISTING GAS FACILITIES.
- RI ENERGY REQUIRES A MINIMUM OF THREE FEET OF SEPARATION BETWEEN THE GAS MAIN AND THE PARALLEL FACILITY FOR STEEL AND PLASTIC GAS MAINS. FOR CAST IRON GAS MAIN SEE LINE ITEM FOR ENCROACHMENT GUIDELINES.
- AT A PROPOSED UTILITY AND CRITICAL GAS MAIN CROSSING, A RI ENERGY GAS DAMAGE PREVENTION INSPECTOR MUST BE ON SITE WHEN CROSSING. CALL ED SOUZA AT 401-283-9159 OR JEFF CASSEL AT 508-468-7217.
- IF A GAS MAIN IS EXPOSED OR GOING TO BE EXPOSED CALL RI ENERGY DISPATCH AT 877-304-1203 FOR AN INSPECTOR TO BE DISPATCHED TO THE SITE TO INSPECT THE LINE BEFORE BACKFILL.
- IF A GAS MAIN OR GAS MAIN COATING IS DAMAGED CALL RI ENERGY DISPATCH OFFICE AT 877-304-1203 FOR AN INSPECTOR TO BE DISPATCHED TO THE SITE FOR REPAIR BEFORE BACKFILL.
- FOR ANY EXPOSED GAS FACILITY, PROVIDE BACKFILL MATERIALS AND COMPACT THE BACKFILL MATERIALS IN ACCORDANCE WITH RI ENERGY'S "GUIDELINES FOR BACKFILL AND COMPACTION AROUND GAS PIPES", DOCUMENT ATTACHED.
- WHEN CROSSING OR EXPOSING A STEEL OR PLASTIC GAS FACILITY SUPPORT MAY BE REQUIRED. FOLLOW THE GUIDELINES LISTED AND ILLUSTRATED IN RI ENERGY'S "SUPPORT REQUIREMENTS FOR EXPOSED & UNDERMINED STEEL OR PLASTIC GAS FACILITIES", DOCUMENT (DWG NO. CNST-6045) ATTACHED.
- ALL GAS VALVE BOXES SHALL BE ADJUSTED TO THE NEW ROAD/SIDEWALK SURFACE. VALVE BOXES, IF REQUIRED FOR REPLACEMENT, CAN BE OBTAINED AT RI ENERGY'S PROVIDENCE LOCATION, 477 DEXTER STREET, PROVIDENCE, RI OR LINCOLN LOCATION, 642 GEORGE WASHINGTON HIGHWAY (QUANTITIES 5 OR LESS). GAS VALVE BOXES NEED TO BE ACCESSIBLE AT ALL TIMES TO BE OPERATED BY RI ENERGY IN THE EVENT OF AN EMERGENCY.
- ALL CATHODIC PROTECTION BOXES (BOXES THAT CONTAIN WIRES THAT GO DOWN TO THE GAS MAIN) SHALL BE ADJUSTED TO THE NEW ROAD/SIDEWALK SURFACE. CARE SHALL BE EXERCISED WHEN ADJUSTING SO AS NOT TO DAMAGE THE WIRES. IF THE WIRES ARE DAMAGED OR IF ASSISTANCE IS NEEDED, CONTACT RI ENERGY CORROSION ENGINEER TO VISIT THE SITE. CONTACT BUTCH VINCENT 508-838-4486. NEW BOXES, IF REQUIRED, CAN BE OBTAINED AT RI ENERGY'S PROVIDENCE FACILITY, 477 DEXTER ST, PROVIDENCE, RI OR RI ENERGY'S LINCOLN FACILITY, 642 GEORGE WASHINGTON HIGHWAY, LINCOLN, RI (QUANTITIES 5 OR LESS). CONTRACTOR SHALL FOLLOW THE GUIDELINES LISTED IN RI ENERGY'S "GUIDELINES FOR WORKING AROUND CORROSION CONTROL SYSTEM COMPONENTS", DOCUMENT ATTACHED.
- DUE TO SYSTEM RELIABILITY AND PUBLIC SAFETY CONCERNS, IT IS RI ENERGY'S PRACTICE TO RESTRICT ALL CONSTRUCTION WORK ON OR NEAR GAS FACILITIES BETWEEN NOVEMBER 15TH AND APRIL 15TH. ALL SCHEDULED WORK SHOULD BE COMPLETED BETWEEN APRIL 15TH AND NOVEMBER 15TH. AS GAS USAGE PEAK DURING THE MONTHS OF DECEMBER TO MARCH DRIVEN BY HEATING NEEDS, RI ENERGY'S PRIORITY IS TO PROVIDE OUR CUSTOMERS WITH SAFE AND RELIABLE GAS SERVICE. ANY WORK ON OR NEAR THE GAS FACILITY WILL EXPOSE OUR CUSTOMERS TO UNNECESSARY RISK. EXCEPTIONS WILL BE CONSIDERED ON A CASE BY CASE BASIS. APPROVALS FROM GAS CONTROL, OPERATIONAL ENGINEERING, AND PROJECT ENGINEERING WILL BE REQUIRED FOR THESE CASES.
- FOR A GAS LEAK CALL 800-640-1595.
- FOR A DAMAGED GAS FACILITY CALL 800-870-1664.

ABANDONED GAS MAIN

- RI ENERGY WILL PURGE OUR OLD GAS MAIN OF GAS, WIPE TEST SAMPLE THE INSIDE OF THE PIPE, CAP THE ENDS AND ABANDON IN PLACE. PIPE FOUR INCHES AND LESS IN DIAMETER CAN'T BE SAMPLED; THIS PIPE WILL BE ASSUMED TO BE CONTAMINATED. IF THE WIPE TEST RESULTS SHOW PCB CONTAMINATION AND A SECTION OR SECTIONS NEED TO BE REMOVED BY THE CONTRACTOR THEN THE CONTRACTOR WILL NEED TO TRANSPORT THE REMOVED SECTIONS WITH THE ENDS SEALED WITH PLASTIC AND ZIP TIES TO EITHER OUR ALLENS AVE FACILITY AT 642 ALLENS AVE IN PROVIDENCE OR OUR DEXTER ST FACILITY AT 477 DEXTER ST IN PROVIDENCE OR OUR LINCOLN FACILITY AT 642 GEORGE WASHINGTON HWY IN LINCOLN AND PLACE THEM IN OUR RED OPEN TOP "PIPE TO BE CLEANED" CONTAINER ON SITE OR ARRANGE FOR CLEAN HARBORS TO DELIVER AN OPEN TOP CONTAINER TO THE SITE FOR THE REMOVED SECTIONS OF PIPE AND TRANSPORTED BACK TO CLEAN HARBORS. RI ENERGY WOULD THEN HANDLE THE CLEANING AND PROPER DISPOSAL. RI ENERGY ALSO REQUIRES THAT THE OPEN PIPE ENDS OF THE ABANDONED PIPE REMAINING IN THE GROUND BE SEALED WITH EXPANDING FOAM. IF THE WIPE TEST SHOWS THAT THE PIPE HAS NO PCB CONTAMINATION THEN REMOVED SECTIONS CAN JUST BE DISPOSED OF BY THE CONTRACTOR AS SCRAP METAL.

TYPICAL SIGN DESIGNATION SYMBOL

-  SIGN NO.
 -  MUTCD SIGN DESIGNATION NUMBER (SIZE)
 -  SIGN MOUNTING:
- 24.2.0 = R.I. STD. 24.2.0



RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
OFFICE OF WATER RESOURCES
FRESHWATER WETLANDS PROGRAM
APPROVED WITH CONDITIONS AS
SPECIFIED IN THE LETTER OF APPROVAL
DATED: JUN 18 2024 FILE #: 24-0152
NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
APPROVED PLANS MUST BE AT CONSTRUCTION SITE
Nancy L. Freeman

 1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE:
SHEET: 5
OF: 13

SCALE: NOT TO SCALE

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

CUMBERLAND RHODE ISLAND

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT
JOB SPECIFIC PLAN SYMBOLS,
LEGEND & NOTES

GENERAL NOTES REGARDING TEMPORARY CONSTRUCTION CONDITIONS:

1. DESIGN WIND PRESSURES FOR CONSTRUCTION:

MINIMUM WIND PRESSURES TO BE USED BY THE CONTRACTOR FOR DESIGN DURING THE CONSTRUCTION CONTRACT (WITH THE EXCEPTION OF SIGNS) SHALL BE FROM THE FOLLOWING TABLE:

HEIGHT ABOVE GROUND	WIND PRESSURE (PSF)
UP TO 17'	33
OVER 17' AND UP TO 33'	37
OVER 33' AND UP TO 50'	41
OVER 50' AND UP TO 75'	44
OVER 75' AND UP TO 100'	47

TABLE NOTES:

A. APPLICATION OF THE TABULAR PRESSURE:

- BRIDGE COMPONENTS DURING CONSTRUCTION, PRIOR TO THE INSTALLATION OF THE PERMANENT BRACING SYSTEMS, NOT INCLUDING CRANE LIFTING.
- FALSE WORK, SHORING, AND SCAFFOLDING AS DEFINED IN FHWA GUIDE DESIGN SPECIFICATION FOR BRIDGE TEMPORARY WORKS, EXCLUDING 3-DIMENSIONAL LATTICED OR TRUSSED FRAMES OR TOWERS;
- TEMPORARY SHIELDING.

WIND PRESSURES FOR ALL OTHER STRUCTURES SHALL BE CALCULATED BASED ON ASCE DESIGN LOADS ON STRUCTURES DURING CONSTRUCTION, SEI/ASCE 37-02 (ALL REFERENCES TO THE ASCE 7 IN THE SEI/ASCE 37-02 PUBLICATION, SHALL BE THE LATEST REVISION OF ASCE 7). THE EXPOSURE CATEGORY SHALL BE C.

B. WHERE APPLICABLE HIGHER AMTRAK WIND REQUIREMENTS SHALL SUPERSEDE THESE REQUIREMENTS.

C. FOR STRUCTURES SITUATED ABOVE LIVE INTERSTATE TRAFFIC, THE TABULAR VALUES SHALL BE INCREASED BY 5 PSF.

2. ERECTION OF BRIDGE COMPONENTS:

FOR THE ERECTION OF STRUCTURES, THE FOLLOWING SHALL APPLY:

- THE CONTRACTOR SHALL SUBMIT AN ERECTION PLAN THAT PROVIDES COMPLETE DETAILS OF THE PROCESS INCLUDING, BUT NOT LIMITED TO, TEMPORARY SUPPORTS, SCHEDULING AND OPERATION SEQUENCING, CRANE PLACEMENT, AND ASSUMED LOADS AND CALCULATED STRESSES DURING VARYING STAGES OF LIFTING. THIS APPLIES TO STRUCTURES OF ANY KIND. THE CAPACITY OF THE CRANE AND ALL LIFTING AND CONNECTING DEVICES SHALL BE ADEQUATE FOR 125 PERCENT (150 PERCENT OVER AMTRAK) OF THE TOTAL PICK LOAD INCLUDING SPREADERS, RIGGING, HOOKS, AND ALL OTHER MATERIALS. THIS FACTOR OF SAFETY SHALL BE IN ADDITION TO ALL MANUFACTURERS' PUBLISHED FACTORS OF SAFETY.
- A REGISTERED PROFESSIONAL ENGINEER, LICENSED IN THE STATE OF RHODE ISLAND, WILL BE REQUIRED TO STAMP THE CONTRACTOR'S ERECTION PLAN.
- THE CONTRACTOR'S PROFESSIONAL ENGINEER WILL BE REQUIRED TO INSPECT AND PROVIDE WRITTEN APPROVAL OF INSTALLATION, PRIOR TO ALLOWING VEHICLES OR PEDESTRIANS ON OR BELOW THE STRUCTURE. THE PROFESSIONAL ENGINEER MUST ALSO STAMP ALL CHANGES TO THE CONTRACTOR'S ERECTION PLAN. ADDITIONALLY, ALL PROPOSED CHANGES MUST BE SUBMITTED TO RIDOT FOR REVIEW AND APPROVAL PRIOR TO IMPLEMENTATION.
- A MANDATORY PRE-ERECTION CONFERENCE WILL BE HELD AT LEAST TWO WEEKS PRIOR TO THE START OF THE GIRDER INSTALLATION TO DISCUSS THE PLAN AND PROCEDURES, WORK SCHEDULES, CONTINGENCY PLANS, SAFETY REQUIREMENTS AND TRAFFIC CONTROL. THE CONTRACTOR'S PROFESSIONAL ENGINEER AND ERECTION SUBCONTRACTOR WILL BE REQUIRED TO ATTEND THIS MEETING, AS WILL THE RIDOT RESIDENT ENGINEER, THE DESIGN PROJECT ENGINEER AND THE DESIGN CONSULTANT. BASED UPON DISCUSSIONS AT THIS MEETING AND A REVIEW OF THE CONTRACTOR'S ERECTION PLAN, RIDOT MAY ORDER THE CONTRACTOR TO MODIFY AND RESUBMIT THE ERECTION PLAN TO THE ENGINEER FOR REVIEW AND APPROVAL.
- THE CONTRACTOR WILL BE REQUIRED TO PERFORM DAILY INSPECTIONS OF THE ERECTED GIRDERS UNTIL THE BRIDGE DECK IS COMPLETELY POURED.
- THE COST OF PREPARING AND STAMPING THE ERECTION PLAN, COMPUTATIONS, AND REPORTS, RESPONDING TO RIDOT'S COMMENTS AND MAKING THE NECESSARY REVISIONS, AND ATTENDANCE AT MEETINGS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE SUPERSTRUCTURE PAY ITEM, BE IT CONCRETE, STEEL OR TIMBER.

3. TEMPORARY BARRIERS ON BRIDGE:

TEMPORARY BARRIERS AND CRASH CUSHIONS TO BE UTILIZED ON THE BRIDGE AND IT'S APPROACHES DURING CONSTRUCTION SHALL MEET TEST LEVEL TL-3 (MASH 2016).

CONSTRUCTION NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION, CONSTRUCTION, OPERATION AND SAFETY OF ALL EQUIPMENT AND PROCEDURES.
- THE CONTRACTOR SHALL SUBMIT WORKING DOCUMENTS SHOWING PROPOSED METHODS OF LIFTING, SEQUENCING OF LIFTING, LOCATION OF CRANES, CRANE CAPACITIES, LOCATION OF THE LIFTING POINTS ON THE BRIDGE COMPONENTS, WEIGHTS OF THE COMPONENTS, LIFTING DEVICES AND LOAD DISTRIBUTION DEVICE DETAIL. THE METHOD AND ALL SUBMISSIONS SHALL BE PREPARED AND STAMPED BY A RHODE ISLAND REGISTERED PROFESSIONAL ENGINEER.
- COORDINATE ALL CONSTRUCTION ACTIVITIES WITHIN THE WORKING AREA WITH RIDOT REGARDING UTILITIES, PROTECTION OF TRAFFIC AND SCHEDULE.
- THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO AVOID DAMAGE TO EXISTING STRUCTURES. ALL STRUCTURES DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER IN SUFFICIENT TIME TO PERMIT CAREFUL CHECKING AS NOT TO DELAY THE PROJECT.
- ALL RIGGING IS TO BE IN EXCELLENT WORKING CONDITION.
- UNLOADED CRANES ARE ALLOWED TO TRAVEL IN THE WORKING AREA.
- CRANE DELIVERY LOCATIONS MAY VARY AS LONG AS MAXIMUM CRANE RADIUS IS NOT EXCEEDED.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO ANCHOR HEAVY EQUIPMENT TO THE EXISTING BRIDGE DURING CONSTRUCTION.
- TEMPORARY EXCAVATION SUPPORT SYSTEM SHALL BE DESIGNED, FURNISHED AND INSTALLED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO THE START OF ANY EXCAVATION.
- CONTRACTOR SHALL SECURE ALL WORK AREAS AT ALL TIMES TO PREVENT UNAUTHORIZED ACCESS.
- STOCKPILED SOIL SHALL BE NOT CLOSER THAN 30 FEET FROM PIERS, WALLS AND ABUTMENTS.
- NO PAYMENT SHALL BE MADE FOR REMOVING AND RESETTING TEMPORARY BARRIERS FOR THE PURPOSE OF GAINING ACCESS AND SPACE FOR WORK AREA, AND SHALL BE CONSIDERED INCIDENTAL TO THE COST OF CONCRETE TEMPORARY BARRIER. NO PAYMENT SHALL BE MADE FOR REMOVING, RELOCATING AND RESETTING ANY BARRIERS MOVED FOR THE CONVENIENCE OF THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT BE ALLOWED TO USE RIVER WATER DURING CONSTRUCTION. IN ADDITION, DISCHARGE FROM THE CONSTRUCTION ACTIVITIES SHALL NOT BE PERMITTED.

UTILITY NOTES:

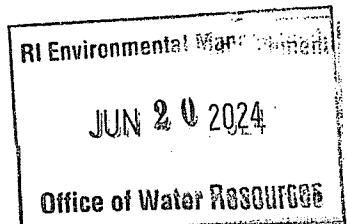
- EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE LOCATED USING THE BEST AVAILABLE INFORMATION. NO BUILDING SERVICE CONNECTIONS (ELECTRIC, TELEPHONE, GAS, WATER, SANITARY AND OTHERS) ARE SHOWN. THE CONTRACTOR IS TO ASSUME THAT SERVICES TO ALL BUILDINGS ARE PRESENT.
- BOTH FEDERAL AND STATE LAW (RI. GENERAL LAW 39-1.2) REQUIRE NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BLASTING, DEMOLISHING, BORING, BACKFILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE STATE AND/OR THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE STATE.
- CONSTRUCTION EQUIPMENT OR PERSONNEL SHALL FOLLOW OSHA REGULATION IN REGARDS TO MINIMUM CLEARANCE TO ENERGIZED OVERHEAD LINES.
- UNDERGROUND UTILITY LINES MAY BE IN CONFLICT WITH REQUIRED TEMPORARY OR PERMANENT CONSTRUCTION, OR THE EQUIPMENT NECESSARY TO PERFORM THIS REQUIRED CONSTRUCTION. DEPENDING UPON THE CONTRACTOR'S METHOD OF CONSTRUCTION, THESE UTILITIES MAY NEED TO BE RELOCATED FOR PORTIONS OF THE CONSTRUCTION PERIOD AND THEN MOVED BACK TO PERMANENT LOCATIONS WHICH MAY BE OTHER THAN CURRENT LOCATIONS. THE ACTUAL RELOCATIONS (TEMPORARY OR PERMANENT) ARE THE RESPONSIBILITY OF THE INDIVIDUAL UTILITY OWNER. HOWEVER, THE CONTRACTOR WILL BE REQUIRED TO COORDINATE THE EXACT LOCATION AND TIMING OF ALL UTILITY RELOCATIONS WITH THE INDIVIDUAL UTILITY OWNER, AND TO PHASE HIS CONSTRUCTION OPERATIONS AS REQUIRED TO ACCOMMODATE ALL (TEMPORARY AND PERMANENT) UTILITY RELOCATIONS. IN ADDITION TO FIELD MEETINGS AND CORRESPONDENCE, THIS COORDINATION MAY INCLUDE STAKING OF LOCATIONS, EXCAVATION AND TEMPORARY GRADING, PROVIDING ACCESS TO EXISTING AND FUTURE UTILITY POLE AND CONDUIT LOCATIONS, OR OTHER PHYSICAL WORK AS REQUIRED TO ALLOW FOR UTILITY RELOCATION WORK. THE CONTRACTOR SHALL ENGAGE IN THE NECESSARY COORDINATION OF UTILITY RELOCATIONS AND ASSOCIATED WORK AT NO ADDITIONAL COST TO THE PROJECT OR THE STATE, AND SHALL HAVE NO RIGHT TO ADDITIONAL COMPENSATION FOR DELAYS OR STAGING AND PHASING OF HIS WORK AS A RESULT OF UTILITY RELOCATION WORK.

TEMPORARY CONSTRUCTION STRUCTURAL STEEL NOTES:

- STRUCTURAL STEEL SHAPES AND PLATES SHALL CONFORM TO THE LATEST PROVISIONS OF THE ASTM DESIGNATION A 709 GRADE 36 OR AS DESIGNATED ON THE PLANS.
- HIGH STRENGTH STEEL BOLTS SHALL CONFORM TO ASTM DESIGNATION A F3125 GRADE A325. THE CONTRACTOR SHALL REFER TO SECTION 824 "CONNECTIONS USING HIGH STRENGTH BOLTS" OF THE STATE OF RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (1997) FOR MATERIAL AND INSTALLATION REQUIREMENTS. ALL HIGH STRENGTH BOLTS SHALL BE 7/8" DIAMETER ON 15/16" DIAMETER HOLES UNLESS OTHERWISE NOTED.
- WASHERS MEETING ASTM DESIGNATION F 436 ARE TO BE USED OVER ALL HOLES THAT ARE MORE THAN 1/16" IN DIAMETER GREATER THAN THE BOLT DIAMETER AND UNDER ALL PARTS TURNED DURING ASSEMBLY.
- WELDING SHALL BE IN ACCORDANCE WITH THE LATEST STRUCTURAL WELDING CODE ANSI/AASHTO/AWS D1.5-2015 (INCLUDING ALL INTERIMS TO DATE) AND APPLICABLE SUPPLEMENTAL AWS PUBLICATIONS. ALL SHOP CONNECTIONS SHALL BE WELDED AND ALL FIELD CONNECTIONS SHALL BE BOLTED UNLESS OTHERWISE NOTED.
- WELDING ELECTRODES SHALL HAVE THE SAME CORROSION RESISTANCE AS THE BASE METAL.
- NO SHOP FILLET WELD SHALL BE LESS THAN 3/16", UNLESS OTHERWISE SPECIFIED.
- PRIOR TO FABRICATION, ALL MATERIALS SHALL BE BLAST-CLEANED TO AT LEAST SSPC-SP6 TO REMOVE ALL OIL, DIRT, GREASE, MILL SCALE AND OTHER DELETERIOUS MATERIALS FROM THE SURFACES OF THE STEEL TO BE FABRICATED.
- WHEN STEEL DIE STAMPS ARE USED TO IDENTIFY PIECES AND MEMBERS, FABRICATORS SHALL UTILIZE LOW STRESS STAMPS.

SPECIAL ENVIRONMENTAL NOTES:

- THE LIMITS OF WORK SHOWN ON THESE CONTRACT DRAWINGS MUST BE STRICTLY ADHERED TO CONCERNING THE RIVER AND ALL JURISDICTIONAL FRESHWATER WETLAND AREAS.
- DISTURBANCES TO RIVERBED SUBSTRATES AS A RESULT OF A TEMPORARY FLOAT SYSTEM INSTALLATION/USE MUST BE AVOIDED AND MINIMIZED TO THE GREATEST EXTENT PRACTICABLE.
- THE TEMPORARY FLOAT MUST BE TETHERED TO TIE-DOWNS, BLOCKS OR SUCH ON THE RIVERBANK. THE USE OF SPUDS OR OTHER ANCHORING MECHANISMS ON THE RIVERBED ARE PROHIBITED.
- RIVER FLOWS MUST REMAIN UNIMPEDED BY THE TEMPORARY FLOAT SYSTEM, I.E. SUCH THAT RIVER FLOWS CAN PASS BENEATH AND/OR AROUND THE FLOAT. THE FLOAT SYSTEM MUST BE CHECKED CAREFULLY AT THE END OF EACH WORKDAY TO ENSURE IT IS SECURELY FASTENED FOR THE NIGHT OR WEEKEND.
- THE TEMPORARY FLOAT MUST BE REMOVED PROMPTLY ONCE CONSTRUCTION OPERATIONS REQUIRING THE USE OF THE FLOAT ARE COMPLETED.
- THE TEMPORARY FLOAT MUST BE DEPLOYED AND REMOVED VIA CRANE OR SUCH POSITIONED ON THE ROAD SURFACE. ACCESS MUST REMAIN FROM THE HIGHWAY RIGHT-OF-WAY TO AVOID DISTURBANCES IN FRESHWATER WETLAND BUFFERS AND GIVEN THE ABSENCE OF TEMPORARY EASEMENTS TO ALLOW ACCESS TO ADJACENT PROPERTIES.
- OTHER DEBRIS CAPTURE MECHANISMS MUST BE USED BY THE CONTRACTOR AS NEEDED TO ENSURE THAT NO DEMOLITION DEBRIS OR SAWCUTTING SLURRY ENTER THE RIVER.



RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
 OFFICE OF WATER RESOURCES
 FRESHWATER WETLANDS PROGRAM
 APPROVED WITH CONDITIONS AS SPECIFIED IN THE LETTER OF APPROVAL
 DATED: JUN 18, 2024 FILE #: 24-0152
 NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
 APPROVED PLANS MUST BE AT CONSTRUCTION SITE
Nancy L. Freeman

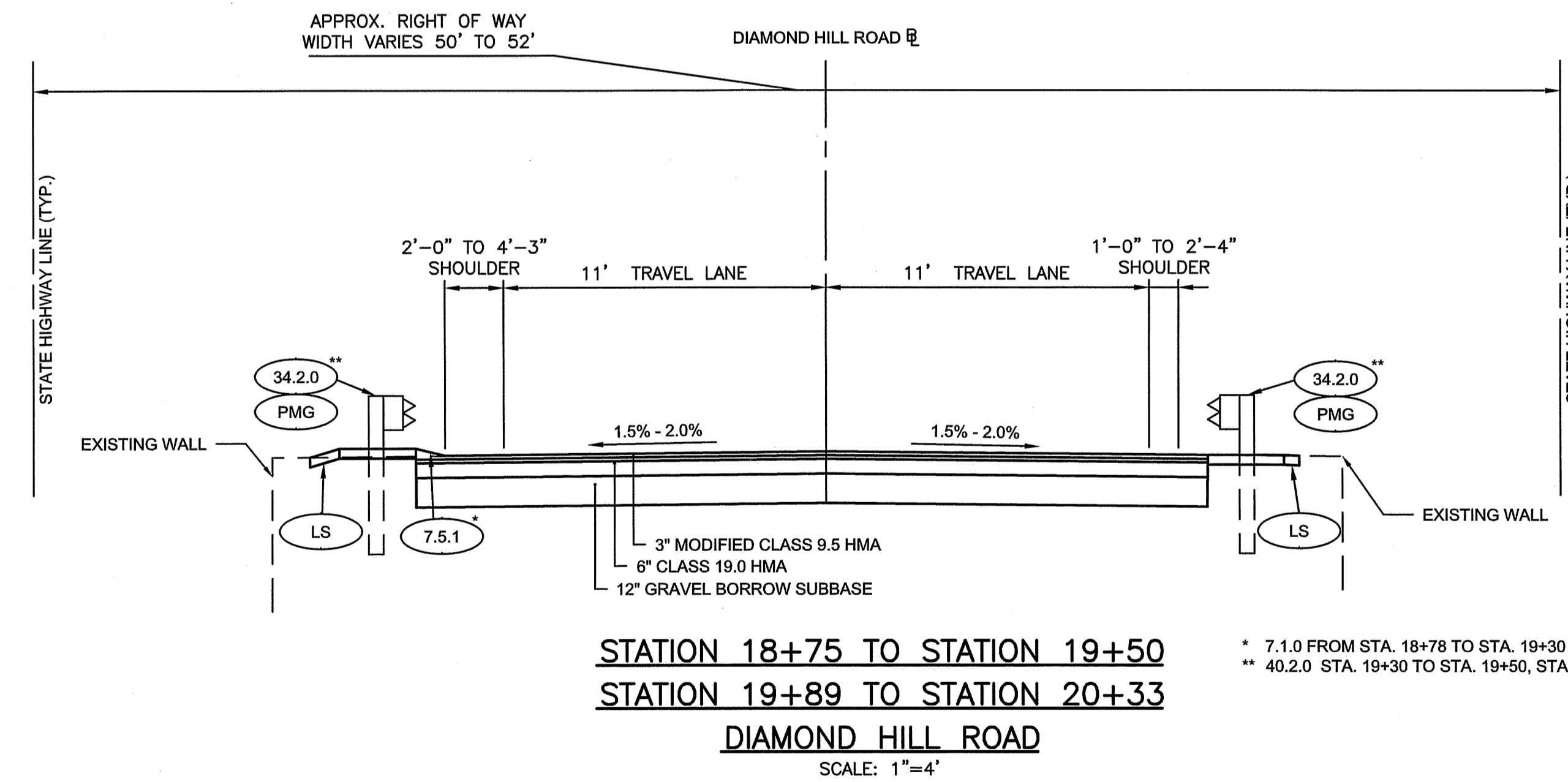
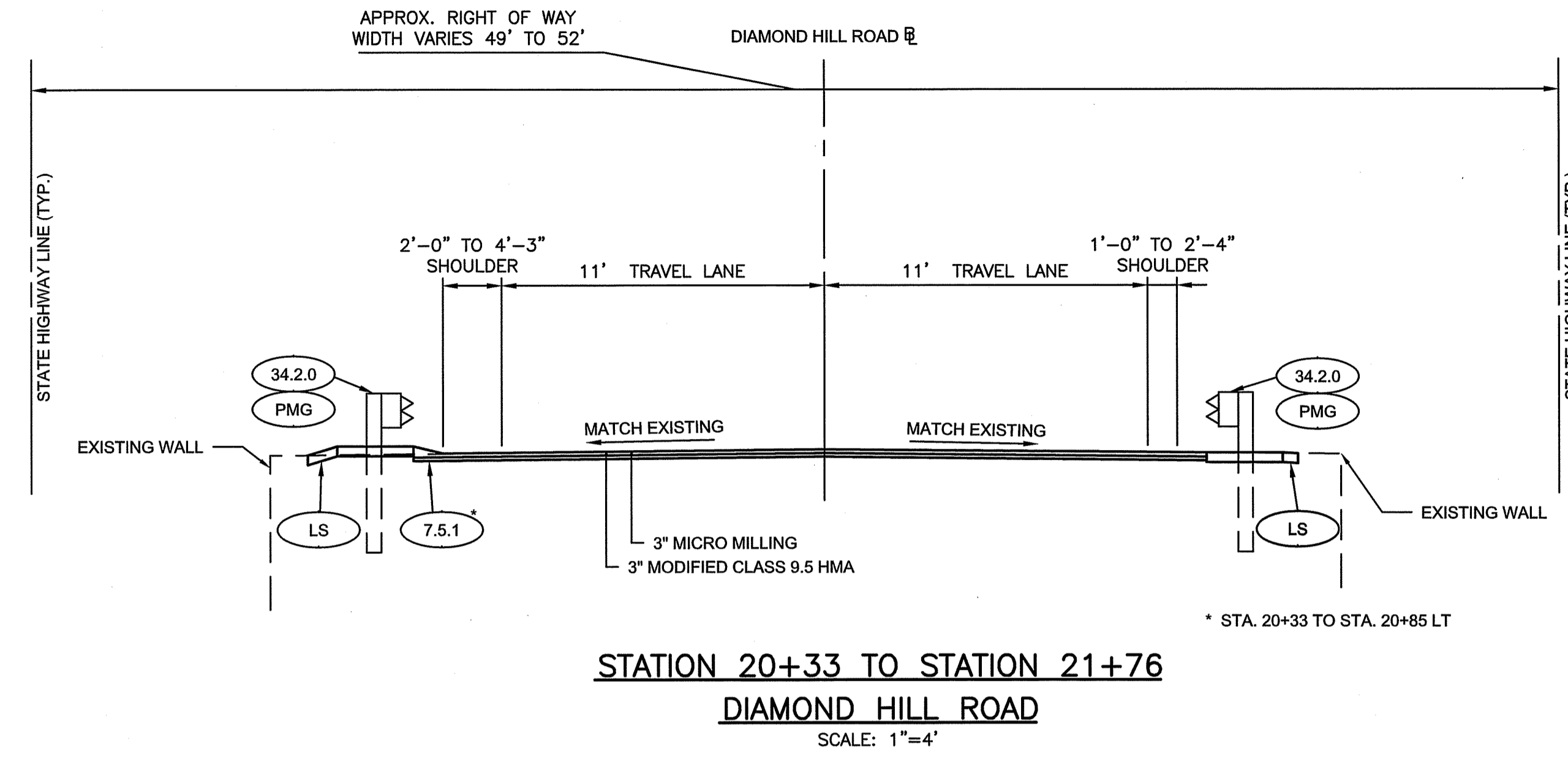


RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
 CHECKED BY:
 DATE:
 SHEET: 6 OF 13

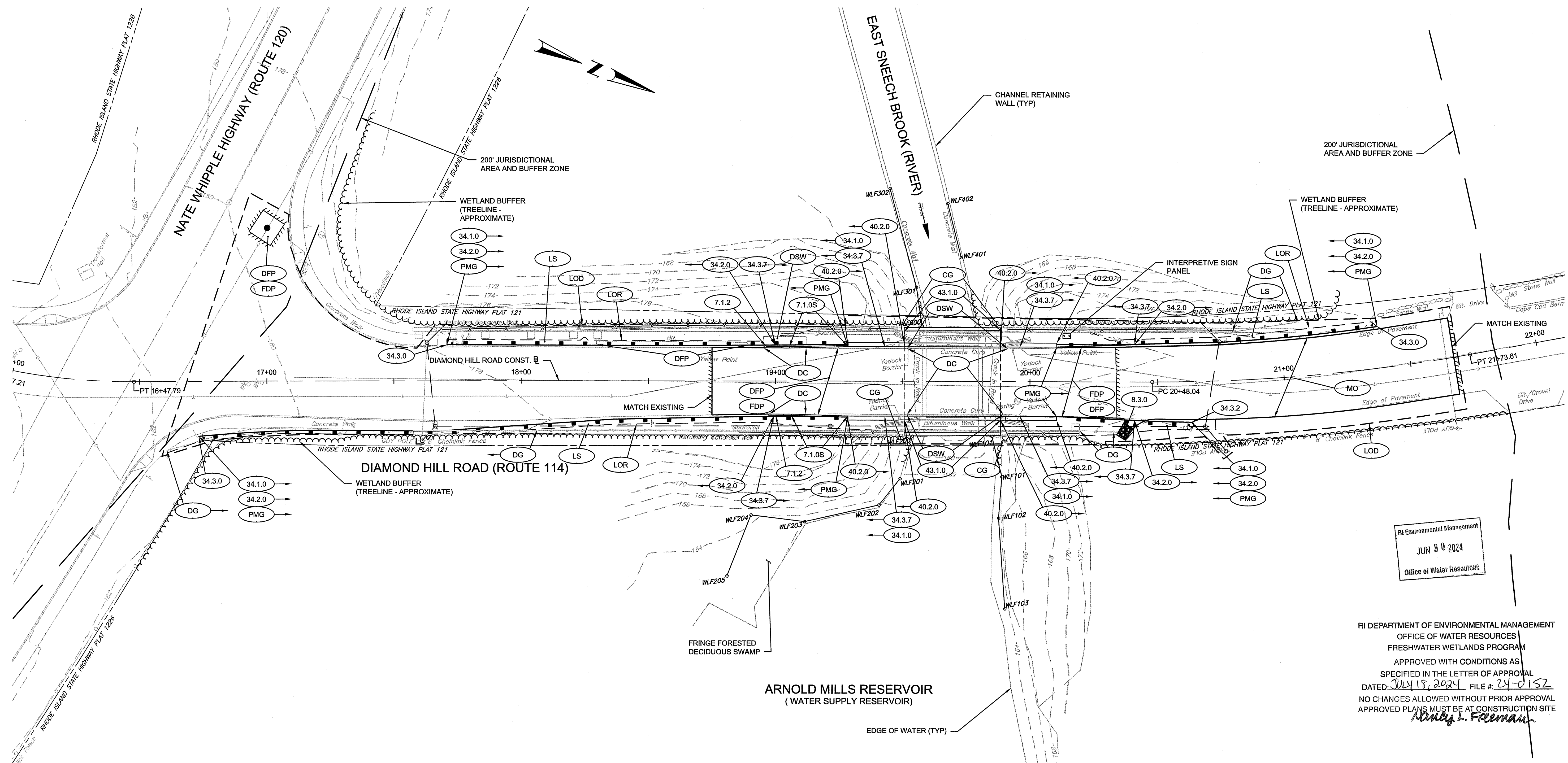
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NO.	DATE	NO.	DATE
5			
43			

BRIDGE GROUP 17C -
 NEWELL BRIDGE REPLACEMENT
 CUMBERLAND
 BRIDGE JOB SPECIFIC NOTES NO. 1
 RHODE ISLAND



RI Environmental Approval
 JUN 20 2024
 Office of Water Resources

RI DEPARTMENT OF ENVIRONMENTAL MANAGEMENT
 OFFICE OF WATER RESOURCES
 FRESHWATER WETLANDS PROGRAM
 APPROVED WITH CONDITIONS AS
 SPECIFIED IN THE LETTER OF APPROVAL
 DATED: JUL 18, 2024 FILE #: 24-0152
 NO CHANGES ALLOWED WITHOUT PRIOR APPROVAL
 APPROVED PLANS MUST BE AT CONSTRUCTION SITE
Nancy L. Freeman



RI Environmental Management
 JUN 20 2024
 Office of Water Resources

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vhb
 1 Cedar Street
 Suite 400
 Providence, RI 02903
 401.272.8100

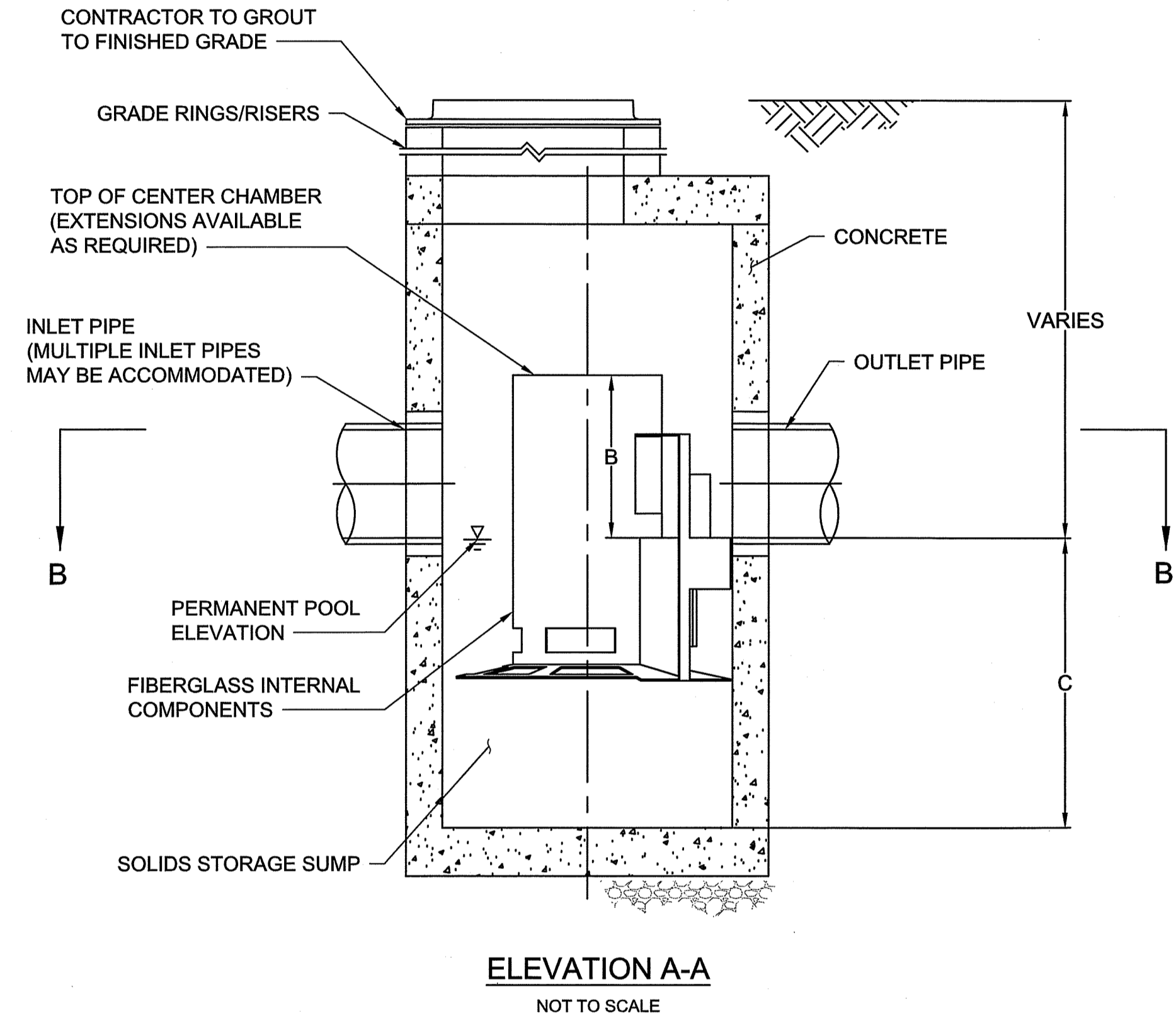
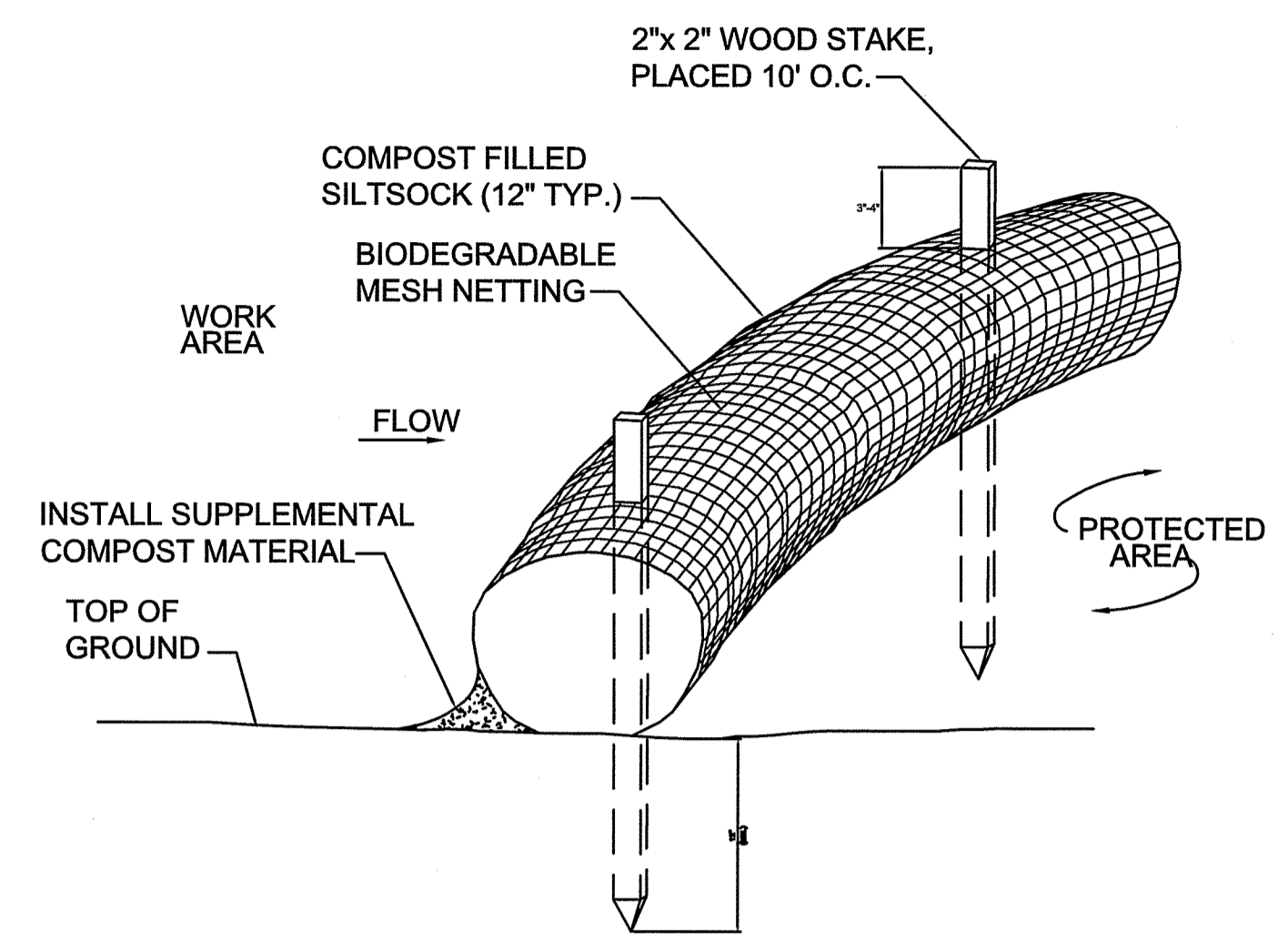
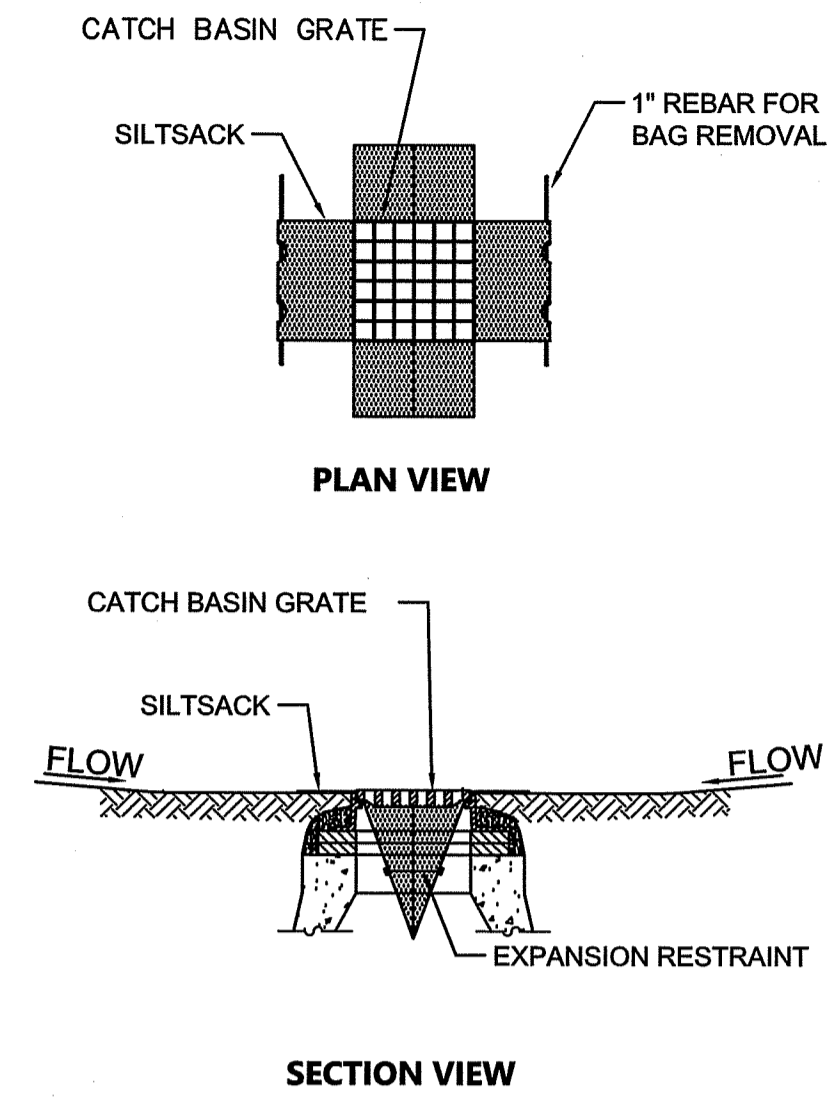
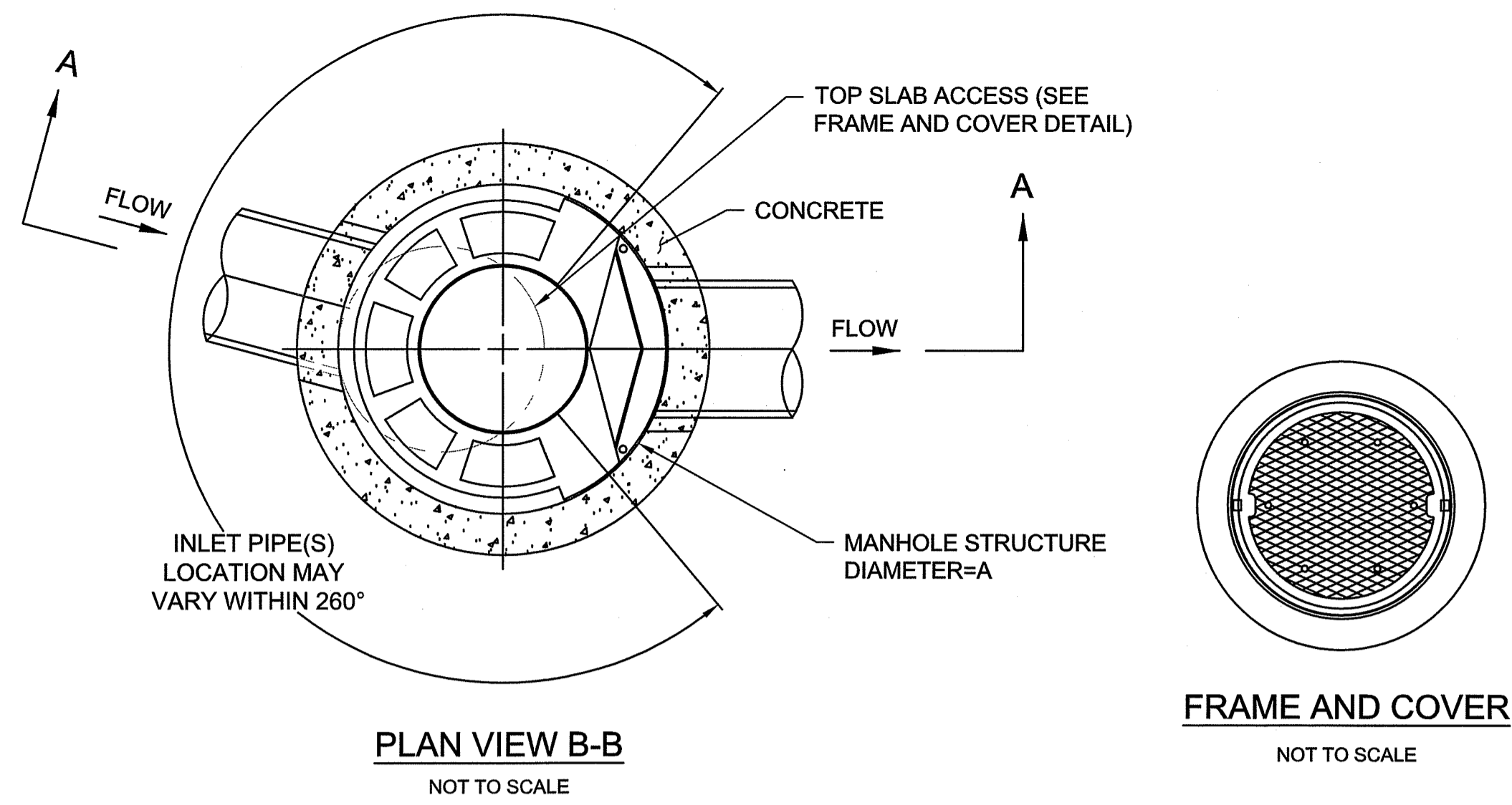
RI DOT
 RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
 CHECKED BY:
 DATE:
 SHEET: 8
 OF: 12/13

SCALE: 1"=20'

SCALE IN FEET					
REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

BRIDGE GROUP 17C -
 NEWELL BRIDGE REPLACEMENT
 CUMBERLAND RHODE ISLAND
GENERAL PLAN NO. 1



DIMENSIONS	
SWTD-1	
A	4'-0"
B	2'-3"
C	4'-0"

- NOTES:
- CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE
 - CONTRACTOR TO PROVIDE, INSTALL, AND GROUT INLET AND OUTLET PIPE(S) THAT ARE BEING INTERCEPTED. IF ANY ADDITIONAL PIPE IS REQUIRED, IT WILL BE CONSIDERED INCIDENTAL TO THE STORM WATER TREATMENT DEVICE.
 - PIPE INVERTS SHALL BE SHOWN TO THE ELEVATIONS SHOWN.
 - ALL PIPE CENTERLINES SHALL MATCH THE PIPE OPENING CENTERLINES.
 - CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO ASSURE EACH UNIT IS WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. ALL JOINTS BELOW PIPE INVERTS SHALL BE GROUTED TO ENSURE EACH STRUCTURE IS WATER TIGHT.

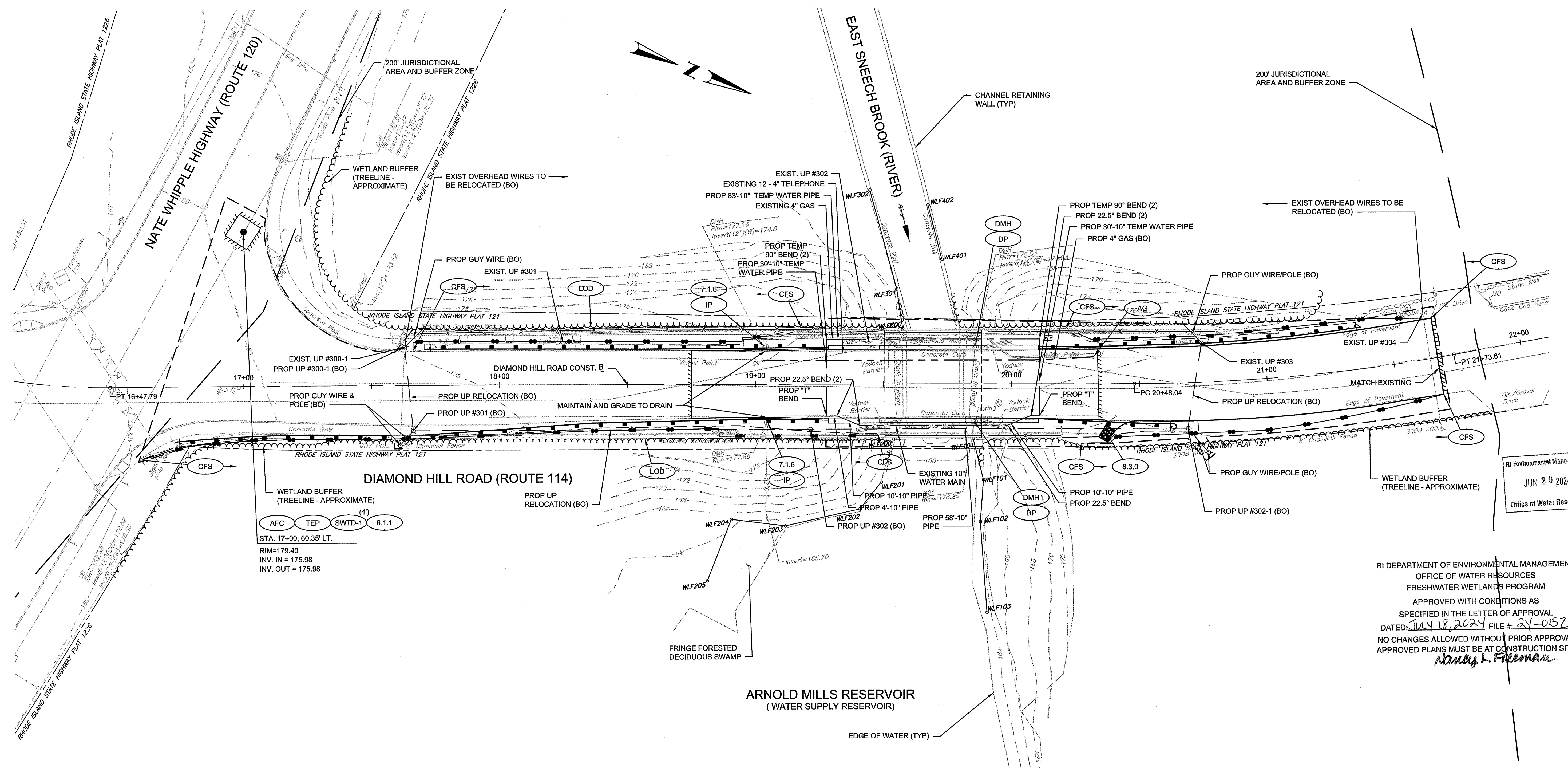
- NOTES:
- INSTALL SILTSACK IN ALL CATCH BASINS WHERE INDICATED ON THE PLAN BEFORE COMMENCING WORK OR IN PAVED AREAS AFTER BINDER COURSE IS PLACED AND HAY BALES HAVE BEEN REMOVED.
 - GRATE TO BE PLACED OVER SILTSACK.
 - SILTSACK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND CLEANING OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED. MAINTAIN UNTIL UPSTREAM AREAS HAVE BEEN PERMANENTLY STABILIZED
- SILT SACK INLET PROTECTION** (IP)
NOT TO SCALE

- NOTES:
- FILTER SOCK SHALL OVERLAP A MINIMUM OF 12 INCHES.
 - FILTER SOCK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS, AND REPAIR OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED.
 - COMPOST MATERIAL SHALL BE DISPERSED ON SITE, AS DETERMINED BY THE ENGINEER.
 - IF NON BIODEGRADABLE NETTING IS USED THE NETTING SHALL BE COLLECTED AND DISPOSED OF OFFSITE.
- COMPOST FILTER SOCK** (CFS)
NOT TO SCALE

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JUN 20 2024
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William J. Freeman

STORM WATER TREATMENT DEVICE (SWTD-1)
NOT TO SCALE



DIAMOND HILL ROAD (ROUTE 114)
 WETLAND BUFFER (TREELINE - APPROXIMATE)
 AFC TEP SWTD-1 (4) 6.1.1
 STA. 17+00, 60.35' LT.
 RIM=179.40
 INV. IN = 175.98
 INV. OUT = 175.98

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Dorothy L. Freeman



1 Cedar Street
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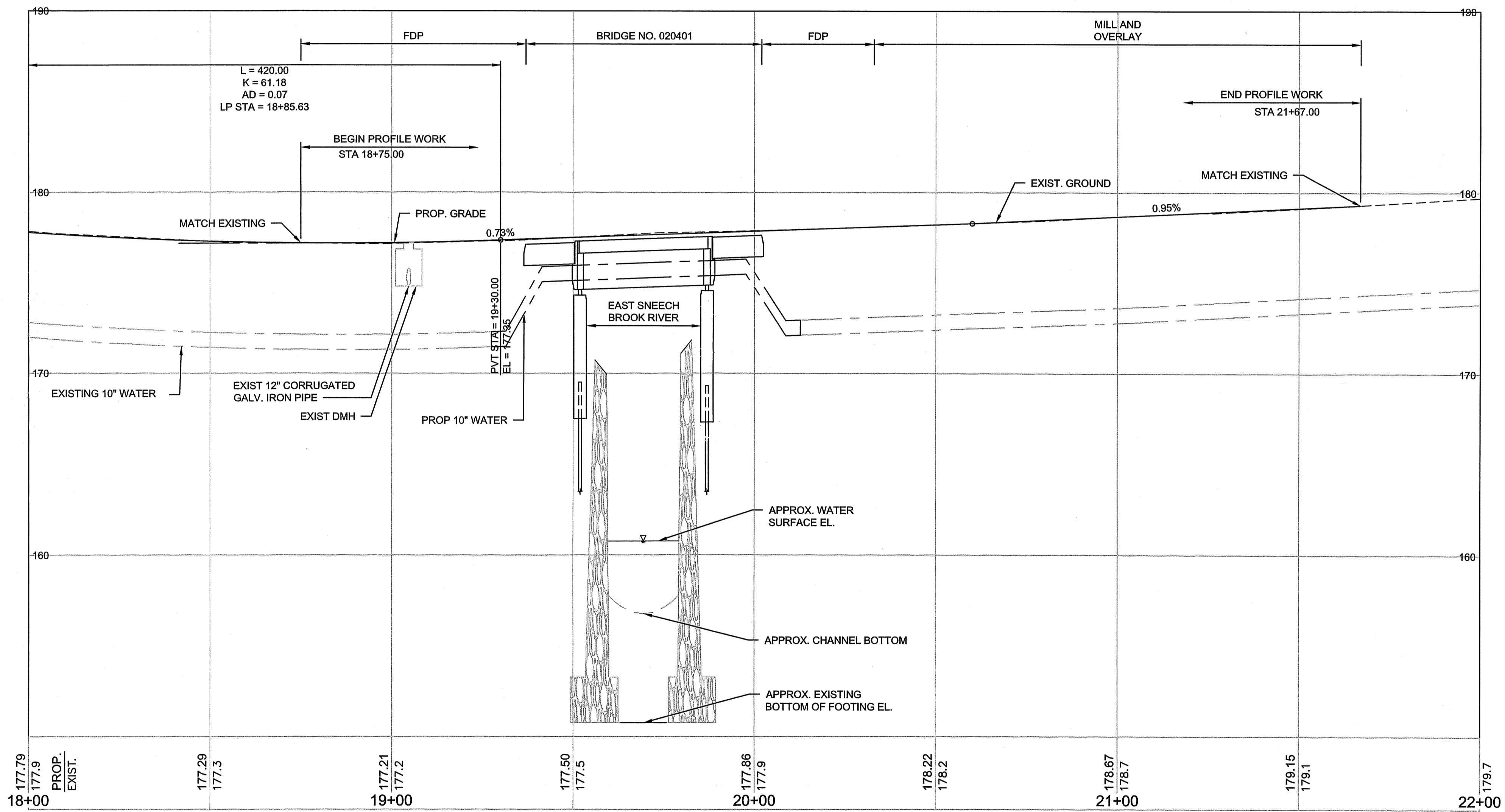
RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
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 DATE:
 SHEET: 10
 OF: 12/13

SCALE: 1"=20'

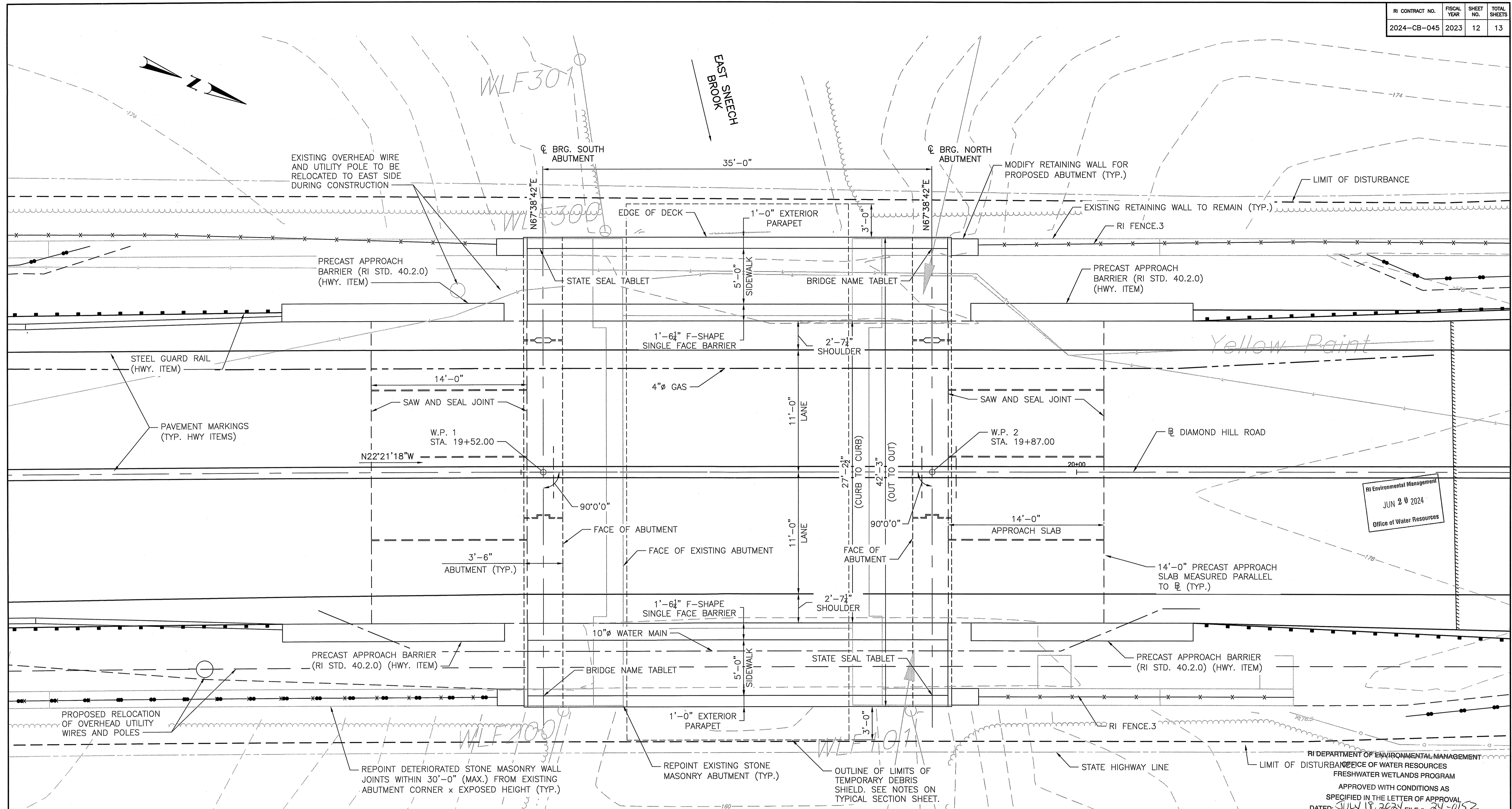
SCALE IN FEET					
REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT
 CUMBERLAND RHODE ISLAND
DRAINAGE & UTILITY PLAN NO. 1



RI Environmental Management
 JUN 20 2024
 Office of Water Resources

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Nancy R. Feldman



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 APPROVED PLAN TO BE AT CONSTRUCTION SITE

- NOTES:**
- REFER TO HIGHWAY PLANS (VOLUME 1) FOR ADDITIONAL DRAINAGE, UTILITY, BARRIER, GUARDRAIL.

BRIDGE GENERAL PLAN
 SCALE: 1/4"=1'-0"

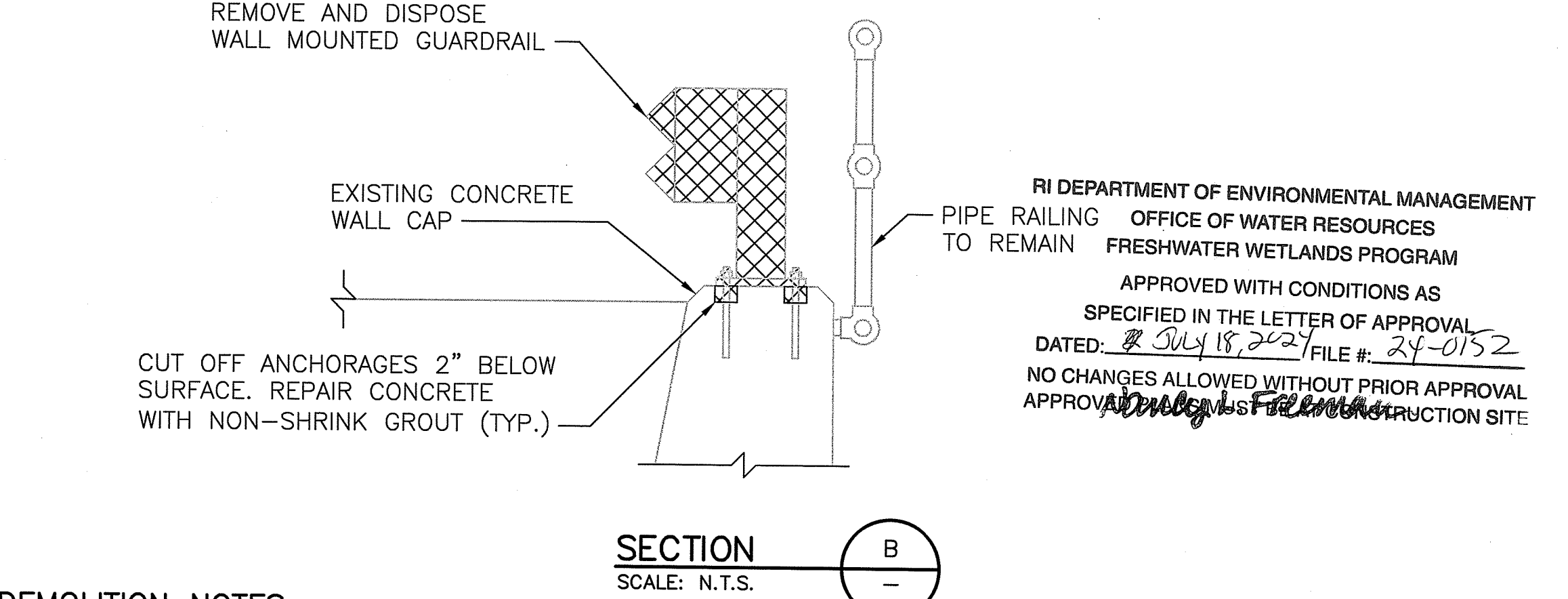
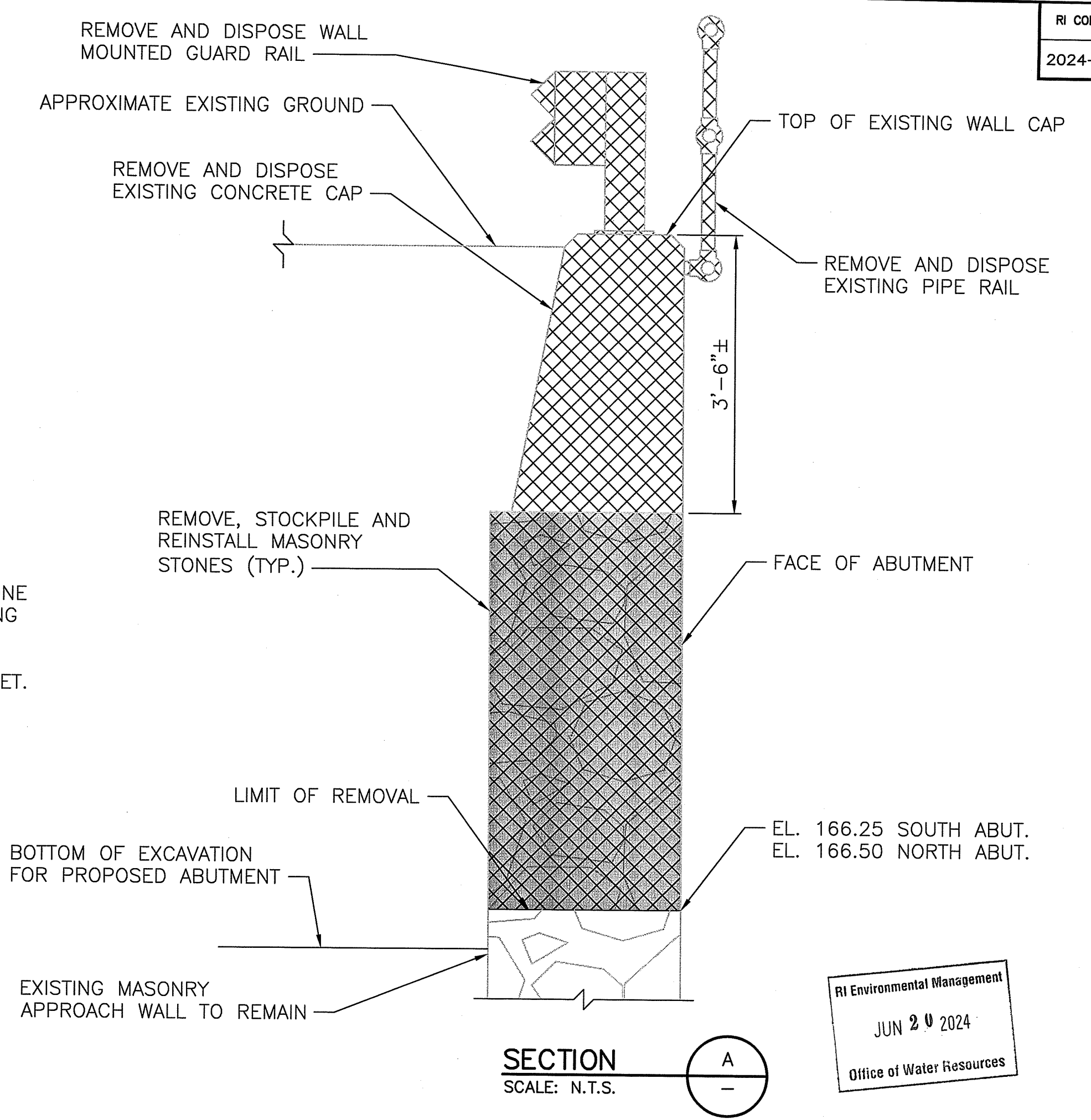
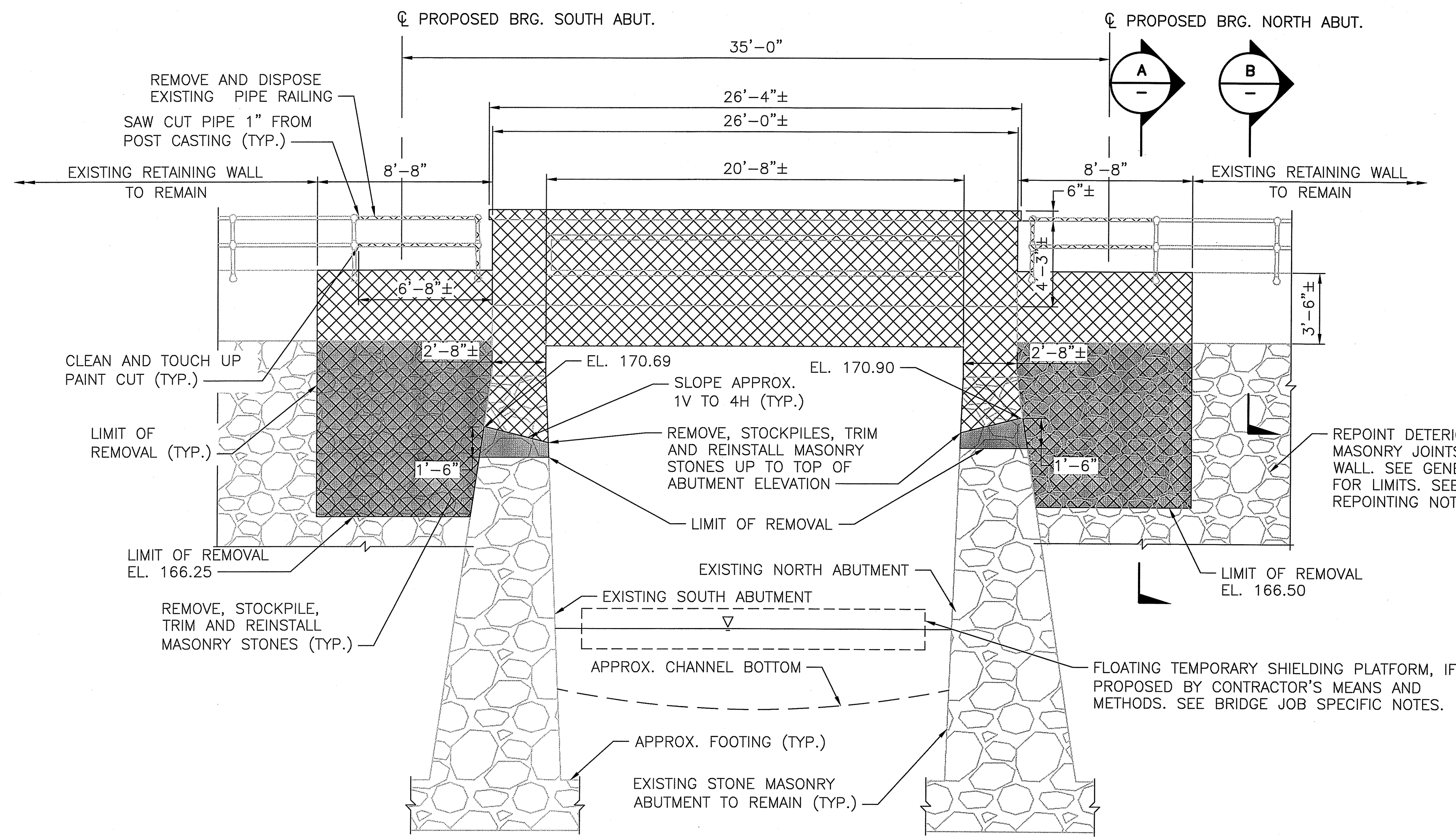
WORKING POINTS			
	STATION	NORTHING	EASTING
W.P. 1	19+52.00	328033.3359	352609.9046
W.P. 2	19+87.00	328065.7055	352596.5925



DESIGNED BY: .
 CHECKED BY: .
 DATE: .
 SHEET: 12 OF 43

REVISIONS			REVISIONS		
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7					
43					

BRIDGE GROUP 17C -
NEWELL BRIDGE REPLACEMENT
 CUMBERLAND RHODE ISLAND
BRIDGE GENERAL PLAN

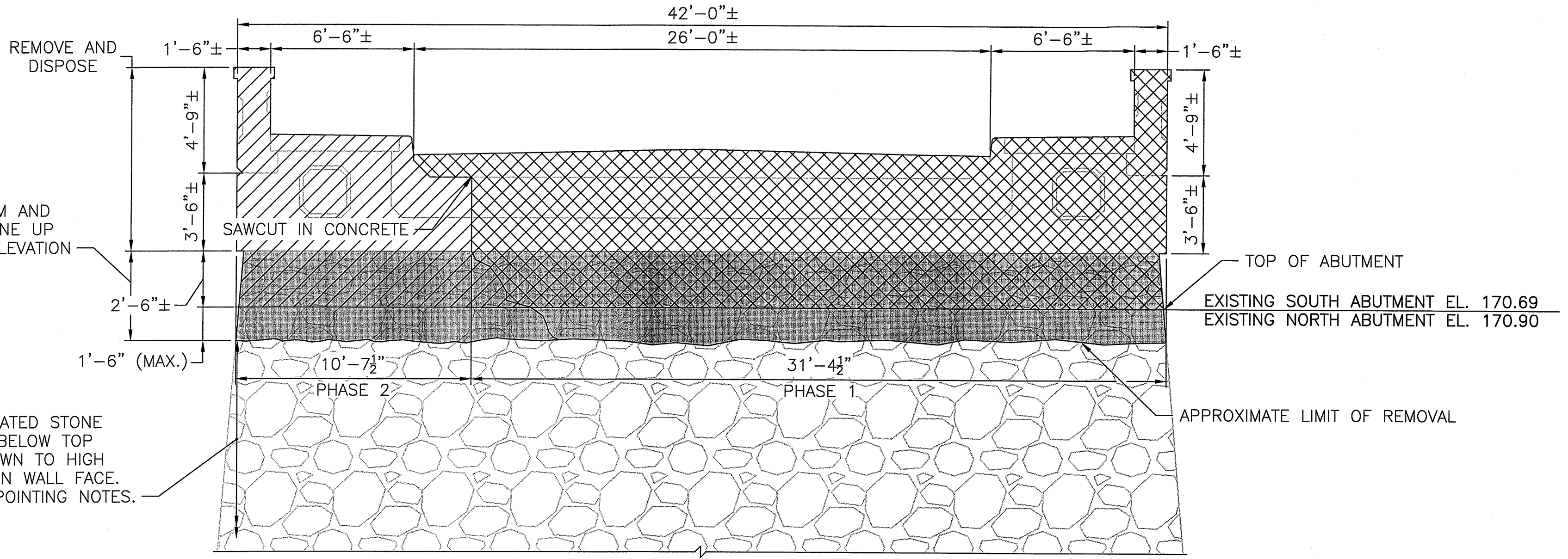


- DEMOLITION NOTES:**
- DIMENSIONS ARE +/- AND HAVE BEEN APPROXIMATED FROM THE RECORD PLANS.
 - SAWCUTS ON THE FACES OF THE ABUTMENTS AND WINGWALLS SHALL BE CONSIDERED INCIDENTAL TO ITEM CODE 803.9902.
 - THE COST TO REMOVE AND DISPOSE BRIDGE RAIL AND CLEANING AND TOUCH UP PAINTING SHALL BE INCLUDED IN ITEM CODE 803.9902 "PARTIAL REMOVE AND DISPOSE EXISTING BRIDGE NO. 204 SUBSTRUCTURE".
 - THE COST TO CUT OFF GUARD RAIL ANCHORAGES 2" BELOW SURFACE AND REPAIR WITH NON-SHRINK GROUT AT THE ANCHORAGE HOLES SHALL BE INCLUDED IN THE COST OF ITEM CODE 803.9902.
 - THE COST TO REMOVE AND STOCK PILE MASONRY STONES SHALL BE INCLUDED IN THE COST OF ITEM CODE 939.0100 "STONE WALLS IN HISTORIC, SCENIC OR RURAL AREAS".
 - FOR ADDITIONAL NOTES SEE BRIDGE JOB SPECIFIC NOTES.

- MASONRY REPOINTING NOTES:**
- STONE MASONRY REPOINTING SHALL BE IN ACCORDANCE WITH SECTION 807 AND SECTION 939 OF THE RIDOT STANDARD SPECIFICATIONS AS APPLICABLE. EXISTING RETAINING WALLS TO REMAIN SHALL BE REPAIRED WITH MORTAR IN ACCORDANCE WITH M04.03.5.
 - MASONRY REPOINTING SHALL INCLUDE REMOVING EXISTING MORTAR FROM STONE JOINTS WITH HAND TOOLS, CHISELS, AND LIGHTWEIGHT CHIPPING EQUIPMENT (MAX. 15LB. CLASS) UP TO 2" DEEP. PREPARE JOINTS IN ACCORDANCE WITH 807.03 AND INSTALL MORTAR AND POINT THE JOINTS. ALL WORK ASSOCIATED WITH REMOVAL AND REPOINTING SHALL BE PAID FOR UNDER ITEM CODE 807.0500 POINTING AND GROUTING.
 - THE CONTRACTOR SHALL PROVIDE DOCUMENTATION OF DETERIORATED STONE MASONRY JOINT LOCATIONS FOR REPAIR. THE CONTRACTOR SHALL PROVIDE ACCESS FOR THE RESIDENT ENGINEER'S REPRESENTATIVE TO FIELD REVIEW THE LOCATIONS AND LIMITS. THE CONTRACTOR SHALL NOT COMMENCE STONE MASONRY REPOINTING UNTIL THE LIMITS HAVE BEEN APPROVED BY THE RESIDENT ENGINEER. ALL WORK ASSOCIATED WITH DOCUMENTATION, ACCESS, REVIEW, AND APPROVAL OF REPOINTING LIMITS SHALL BE INCLUDED IN THE COST OF ITEM COD 807.0500 POINTING AND GROUTING.

- LEGEND:**
- REMOVE AND DISPOSE PHASE 1
 - REMOVE AND DISPOSE PHASE 2
 - REMOVE AND STOCKPILE STONE MASONRY (PHASE 1 SHOWN, PHASE 2 SIMILAR)
 - REMOVE AND REBUILD TO PROVIDE SLOPED TOP SURFACE

NOTE:
EAST ELEVATION (PHASE 1) SHOWN, WEST ELEVATION (PHASE 2) SIMILAR.
EXISTING BRIDGE ELEVATION
SCALE: 1/4"=1'-0"



NOTE:
SOUTH ELEVATION (PHASE 1) SHOWN, NORTH ELEVATION (PHASE 2) SIMILAR.
EXISTING ABUTMENT ELEVATION
SCALE: 1/4"=1'-0"

SCALE:

REVISIONS			REVISIONS		
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